

**MINUTES**  
**BOARD OF BUILDING STANDARDS/  
ARCHITECTURAL BOARD OF REVIEW/SIGN REVIEW BOARD**  
**JANUARY 15, 2015 at 5:30 P.M.**  
**LAKEWOOD CITY HALL**  
**AUDITORIUM**  
**(Audio Recording Available)**

The meeting was called to order at 5:30 P.M.

**1. Roll Call**

MEMBERS PRESENT:

Bryan Evans, Vice-Chairman  
Daniel Musson  
Carl Orban, Chairman  
David Robar  
Cynthia Stockman

OTHERS PRESENT:

Bryce Sylvester, Board Secretary, City Planner  
Michael Molinski, Building Commissioner and City Architect  
Jason Russell, Project Specialist, Planning and Development

**2. Oath of Office**

Daniel Musson was administered the Oath of Office to serve a five year term; term expiring on December 31, 2019

**3. Elect a Chairperson for the year 2015**

A motion was made by Mr. Robar, seconded by Mr. Musson to elect Carl Orban as Chairperson for the year 2015. All of the members voting yea, the motion passed.

**4. Elect a Vice-Chairperson for the year 2015**

A motion was made by Mr. Robar, seconded by Mr. Musson to elect Bryan Evans as Vice-Chairperson for the year 2015. All of the members voting yea, the motion passed.

**5. Approve the minutes of the December 4, 2014 special meeting – Schools**

A motion was made by Mr. Orban, seconded by Mr. Evans to **APPROVE** the minutes of the December 4, 2014 Special Meeting – Schools. All of the members voting yea, the motion passed.

**6. Approve the minutes of the December 11, 2014 meeting**

A motion was made by Mr. Orban, seconded by Mr. Evans to **APPROVE** the December 11 2014 minutes. All of the members voting yea, the motion passed.

**7. Opening Remarks**

There were no Opening Remarks. Mr. Sylvester asked the Board to move item 13, Docket No. 12-118-14 A to the beginning of the meeting as the applicant had a 7:00 p.m. another meeting in Berea.

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The following was read to the public prior to start of business:

Applicants and anyone wanting to make a statement are asked to come to the podium, print and sign your name on the oath sheet and slowly and clearly state your name and address. If you have a business card, please give one to the secretary. Anyone else wishing to address the board is asked to follow the same procedure.



9. Docket No. 09-88-14 C 14200 & 14206 Detroit Avenue  
BTD Lakewood LLC

- Approve Daniel E. Margulies  
 Deny Daniel Margulies Co. Inc.  
 Defer 14900 Detroit Avenue  
Lakewood, Ohio 44107

The applicant requests the review and approval to construct a new commercial structure on the corner of Detroit and Manor Park Avenues. This item was deferred from the meeting of December 11, 2014. (Page 6)

Daniel R. Margulies, applicant, and John Faile, Architect, were present to explain the changes to the request.

Mr. Evans expressed concern for the lack of storm water run-off management, circulation of parking lot vehicular traffic, the maple tree behind the dumpster, and egress onto Manor Park Avenue.

Public comment was taken.

Mr. Robar said the design had come a long way from the first presentation; it was a modern interpretation on a traditional style.

Mr. Sylvester agreed with Mr. Robar. Mr. Molinski said the design interfaced well between the residential and commercial in an urban core; it worked well as a gateway to the neighborhood.

A motion was made by Mr. Orban, seconded by Mr. Robar to **APPROVE** the application with the following stipulations:

- No signage as shown on the proposal is before the Board for approval at the January 15, 2015 meeting,
- The dumpster is moved back as far east as possible,
- A site photometrics plan will be submitted for Administrative review,
- Final detail on the termination of the sidewalk on eastern end in front of the apartments will be submitted for Administrative review,
- Maple tree changed to an arborvitae in the NW corner, and
- No right-turn onto Manor Park Avenue.

Mr. Orban, Mr. Robar and Ms. Stockman voting yea, and Mr. Evans and Mr. Musson voting nay, the motion passed with a three to two vote.

**Items 10 & 14 are called together as they are the same property.**

10. Docket No. 11-110-14 - A C 13714 Madison Avenue  
Kidzenia Learning Center

- Approve Erin Gendt  
 Deny LDA Architects, Inc.  
 Defer 5000 Euclid Avenue, Suite 104  
Cleveland, Ohio 44103

The applicant requests the review and approval for renovation of an existing church to a full service daycare center

with new entry addition. This item was deferred from the meeting of December 11, 2014. (Page 7)

Erin Gendt, applicant was present to explain the changes to the requests. She stated there would be no renovations to the exterior of the church. As a result, Docket No. 11-110-14 – A was withdrawn for consideration. The Board acknowledged its withdrawal.

#### SIGN REVIEW

14. Docket No. 11-110-14 - S 13714 Madison Avenue  
Kidzenia Learning Center

Approve Erin Gendt  
 Deny LDA Architects, Inc.  
 Defer 5000 Euclid Avenue, Suite 104  
Cleveland, Ohio 44103

The applicant requests the review and approval of signage for a new business. This item was deferred from the meeting of December 11, 2014. (Page 7)

There were no comments from the Board. Public comment was closed as there was no one to address the issue.

A motion was made by Mr. Orban, seconded by Mr. Evans to **APPROVE** the application as amended. All of the members voting yea, the motion passed.

**Items 12 & 16 are called together as they are the same property.**

#### ARCHITECTURAL BOARD OF REVIEW

12. Docket No. 12-117-14 - A C 17921-23 Detroit Avenue  
Madonna Enoteca Ristorante

Approve Maggie Gallagher  
 Deny MG Real Estate LLC  
 Defer 12700 Lake Avenue, #2210  
Lakewood, Ohio 44107

The applicant requests the review and approval of proposed awning for a new restaurant. This item was deferred from the meeting of December 11, 2014. (Page 9)

Maggie Gallagher, MG Real Estate LLC, applicant was present to explain the changes to the requests.

Public comment was closed as there was no one to address the issue.

A motion was made by Mr. Orban, seconded by Mr. Evans to **APPROVE** Docket No. 12-118-14 – A with the following stipulation:

- Drawings and bulkhead materials are submitted for administrative review and approval before obtaining the permits.

All of the members voting yea, the motion passed.

#### SIGN REVIEW

16. Docket No. 12-117-14 - S 17921-23 Detroit Avenue  
Madonna Enoteca Ristorante

- Approve
- Deny
- Defect

Maggie Gallagher  
MG Real Estate LLC  
12700 Lake Avenue, #2210  
Lakewood, Ohio 44107

The applicant requests the review and approval of proposed signage for a new restaurant. This item was deferred from the meeting of December 11, 2014. (Page 9)

Maggie Gallagher, applicant stated the request for signage was being withdrawn (Docket No. 12-118-14 S). The Board acknowledged its withdrawal.

#### SIGN REVIEW

15. Docket No. 11-114-14

11922 Madison Avenue  
East End Bistro & Pub

- Approve
- Deny
- Defer

Brandy Maxey  
East End Bistro & Pub  
34399 Park Drive  
Avon, Ohio 44011

The applicant requests the review and approval of the name change to an existing sign. This item was deferred from the meeting of December 11, 2014. (Page 11)

Brandy Maxey, applicant was present to explain the changes to the request.

Public comment was closed as there was no one to address the issue.

A motion was made by Mr. Orban, seconded by Ms. Stockman to **APPROVE** the amended submission with the following stipulations:

- There is no exposed conduit, and
- The taps on the sign are enlarged.

All of the members voting yea, the motion passed.

17. Docket No. 12-119-14

12222 Detroit Avenue  
Bruce's Auto

- Approve
- Deny
- Defer

Bruce R. Henthorn  
4014 West 163<sup>rd</sup> Street  
Cleveland, Ohio 44135

The applicant requests the review and approval of proposed signage for a new business. This item was deferred from the meeting of December 11, 2014. (Page 12)

Mr. Sylvester communicated with the applicant who requested the item be withdrawn as he was still working on a design. The Board acknowledged its withdrawal.

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#### NEW BUSINESS

**ARCHITECTURAL BOARD OF REVIEW**

18. Docket No. 01-05-15

C 14300 Detroit Avenue  
The Westerly Apartments

- Approve
- Deny
- Defer

Brian Granbott  
Herman Gibans Fodor, Inc. - Architects  
1939 West 25<sup>th</sup> Street, Suite 300  
Cleveland, Ohio 44113

The applicant requests the review and approval of site plan, parking lot modification and exterior modifications of the building. (Page 13)

Mr. Sylvester stated the applicant needed to withdraw the current request and would submit a new proposal in the future. The Board acknowledged its withdrawal.

**SIGN REVIEW**

19. Docket No. 01-06-15

14871 Detroit Avenue  
Marco's Pizza

- Approve
- Deny
- Defer

Dean Schramm  
Schramm Signs  
41431 Schadden Road  
Elyria, Ohio 44035

The applicant requests the review and approval of a proposed 3-color channel letter sign. (Page 32)

Dean Schramm, Schramm Signs, applicant was present to explain the request.

Public comment was closed as there was no one to address the issue.

A motion was made by Mr. Orban, seconded by Mr. Evans to **APPROVE** the application as amended. All of the members voting yea, the motion passed.

20. Docket No. 01-07-15

13428 Madison Avenue  
Slone & Co. Funeral Directors

- Approve
- Deny
- Defer

Charles Slone  
Slone & Co. / First Choice  
1455 West 117<sup>th</sup> Street  
Cleveland, Ohio 44102

The applicant requests the review and approval to replace an existing monument sign. (Page 36)

Skip Collins, All Signs and Designs, representative of the applicant was present to explain the request.

Mr. Musson felt the sign face white area glowed too brightly at night, and asked if the sign could be lowered. Mr. Orban wanted the sign upgraded for a look of permanence.

Mr. Martin, Martin Construction said there had been a lot of work to get the signage to its present state; it served as company branding.

Public comment was closed as there was no one to address the issue.

A motion was made by Mr. Orban, seconded by Ms. Stockman to **APPROVE** the application with the following stipulations:

- The sign is lowered to 8 inches above the planter,
- The 60 inch by 24 inch cabinet is made from a wood composite,
- Sign face is made from acrylic raised letters,
- The sign is illuminated externally,
- Thickness/width of the cabinet is reduced to 4 inches, and
- All details will be submitted to the Building Department for administrative review and approval.

All of the members voting yea, the motion passed.

21. Docket No. 01-08-15 11800 Madison Avenue  
Hertz

Approve James Vacey  
 Deny Signature Sign Company  
 Defer 1776 East 43rd Street  
Cleveland, Ohio 44103

The applicant requests the review and approval to replace of replacement faces on pylon sign, channel letters on backer to wall, and vinyl on window and door. (Page 39)

James Vacey, Signature Sign Company was present to explain the request.

In lieu of the pole sign, Mr. Orban encouraged the use of a monument sign. Mr. Robar liked the look of a perforated building sign. Mr. Evans felt there was an excessive amount of yellow with the proposed building sign and did not like the box cabinet. Mr. Orban preferred to retain the channel letters.

Public comment was closed as there was no one to address the issue.

A motion was made by Mr. Orban, seconded by Mr. Evans to **DEFER** the application until the meeting of February 12, 2015. All of the members voting yea, the motion passed.

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**OLD BUSINESS**  
**ARCHITECTURAL BOARD OF REVIEW**

11. Docket No. 11-112-14 C 1381 West Clifton Boulevard  
Former McKinley School Site

Approve Dru Siley, Director of Planning and Development  
 Deny City of Lakewood  
 Defer 12650 Detroit Avenue  
Lakewood, Ohio 44107

The applicant requests the review of approximately 40 single-family townhouses with a combination of first floor living and traditional townhouse design. This item was deferred from the meeting of December 11, 2014. (Page 8)

Dru Siley, P&D Director, applicant was present to explain the request, along with Greg Solis, RDL Architects and the team with Liberty Developers. Final approval was not being sought, they were looking for guidance.

Mr. Evans said there was an issue with the parapets, roofline and gables as they were not working well together. Mr. Orban felt the units were too tall. Although he liked the rooftop access but not the simple look of the doors between units. Ms. Stockman said there were too many contrasting colors. Ms. Stockman wanted more details presented at the next meeting.

Public comment was closed as there was no one to address the issue.

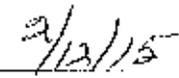
A motion was made by Mr. Orban, seconded by to DEFER the application until February 12, 2015. All of the members voting yea, the motion passed.

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### ADJOURN

A motion was made by Mr. Orban, seconded by Mr. Evans to ADJOURN the meeting at 8:53 p.m. All of the members voting yea, the motion passed.

  
\_\_\_\_\_  
Signature

  
\_\_\_\_\_  
Date



**Oath**

*(You need not give an oath if you object. If you object to giving an oath, please notify the hearing officer or secretary before signing below.)*

I, the undersigned, hereby solemnly swear that the testimony I give at this proceeding will be the truth, the whole truth and nothing but the truth:

PRINT NAME:

SIGN NAME:

- 1. ~~Christopher T. ...~~
- 2. Ken Perrin
- 3. Patrick Siegel
- 4. Tom Corrigan
- 5. Daniel E. ...
- 6. JOHN FAIR
- 7. Sean McEwen
- 8. Ben McKown
- 9. Brenda ...
- 10. ...
- 11. ...

- 
- 
- TOM CORRIGAN
- 
- 
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- 
- 
- David C. ...

Prepared by: The City of Lakewood Law Department, 12650 Detroit Ave., Lakewood, Ohio 44107

FOR CITY USE ONLY

Lakewood Administrative Procedure:  ABR/BBS  Citizens Advisory  Civil Svc.  Dangerous Dog  Income Tax Appeals  Loan Approval  Nuisance Abatement Appeals  Parking  Planning  Zoning Appeals  Other:

Date of Proceeding: Thursday, January 15, 2015



**Oath**

*(You need not give an oath if you object. If you object to giving an oath, please notify the hearing officer or secretary before signing below.)*

I, the undersigned, hereby solemnly swear that the testimony I give at this proceeding will be the truth, the whole truth and nothing but the truth:

PRINT NAME:

SIGN NAME:

1. ERIN GENDT

*Erin Gendt*

2. Brandy Mayer

*Brandy Mayer*

3. Leah Schen

*Leah Schen*

4. SA: A. Collins

*SA: A. Collins*

5. *[Handwritten Name]*

*[Handwritten Signature]*

6. *[Handwritten Name]*

*[Handwritten Signature]*

7. *[Handwritten Name]*

*[Handwritten Signature]*

8. GREGORY S. SOLTIS

*Gregory S. Soltis*

9. \_\_\_\_\_

\_\_\_\_\_

10. \_\_\_\_\_

\_\_\_\_\_

11. \_\_\_\_\_

\_\_\_\_\_

Prepared by: The City of Lakewood Law Department, 12650 Detroit Ave., Lakewood, Ohio 44107

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Lakewood Administrative Procedure:  ABR/BBS  Citizens Advisory  Civil Svc.  Dangerous Dog  Income Tax Appeals  Loan Approval  Nuisance Abatement Appeals  Parking  Planning  Zoning Appeals  Other:

Date of Proceeding: Thursday, January 15, 2015

## Schwarz, Johanna

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**From:** gerrikodyGK <gerrikody.gk@gmail.com>  
**Sent:** Monday, January 05, 2015 8:58 AM  
**To:** Planning Dept  
**Subject:** "McKinley school..redevelopment .."

Hello. I am a 2.5 yr resident in a west clifton facing apartment bldg across street from proposed development site. I am also an artist in my 50 s that enjoys the neighborhood and the pace of things here. I understand a construction site may be inevitable; but my greatest hopes and wishes are that it's kept as low key in terms of noise first and foremost; and next appearance... Is it possible to erect a green view - blocking type fence...? Also working only in daytime / weekday hr.s truly helps... When they demolished bldg last summer; it was livable as we knew it wasn't long days every day! Plz remember we are part of bigger picture of quality neighborhood for you future tenants. Hoping to keep this side of the street quality rentals! Thank you.

Sincerely G.K.

Sent on the new Sprint Network from my Samsung Galaxy SIII.

## Sylvester, Bryce

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**From:** Sean McDermott <smcdermott1490@gmail.com>  
**Sent:** Wednesday, January 14, 2015 11:16 PM  
**To:** Sylvester, Bryce  
**Subject:** Dominos - Manor Park

Bryce,

Thanks for sending the revised development plans for the Dominos project. I'd like to share a few observations and comments:

In general, the plans still leave a TREMENDOUS amount of detail to be assumed and I think set a very bad precedent if approved.

- The proposed one-way configuration of the parking lot is awkward and dangerous. The acute angle of the driveway exit to Manor Park is counter-intuitive and will result in head-on vehicular conflicts. People WILL enter at the proposed exit location if built as configured. Perhaps the one-way arrangement would work in a clock-wise direction.

- The proposed location of the dumpster enclosure is not desirable (in my opinion). The enclosure has been proposed in a very visible location, nearly in the front yard of the neighboring apartment building. I can't think of another modern development project in Lakewood that has permitted a dumpster in a comparable visible location. Additionally, since no grades are shown on any plans, the applicant is not demonstrating just how tall the dumpster enclosure will be, being that the backside of the enclosure facing Manor Park will have an exposed or extended foundation stem wall due to the grades. This will become extremely unattractive, even if screened with arborvitae.

It is obvious that stormwater considerations are an afterthought on the plan. They provide the note:

*NOTE: WATER DETENTION WILL BE UNDERGROUND THROUGH USE OF LARGER PIPING WITH CONTROLLED DISCHARGE. WATER QUALITY STANDARDS WILL BE MET THROUGH THE USE OF A FILTER BAG SYSTEM IN THE CATCH BASINS!*

I understand that detailed stormwater analysis will be performed post ABR, but their indication of what is to be provided leaves questions (i.e. "larger" pipes and filter bags). Most structural BMP's (like filter bag systems in catch basins) are not meant for long term post construction stormwater management, and it is unrealistic to expect proper perpetual maintenance of what is being proposed. The local office of the Ohio EPA has a process for approving structural BMP's (and it's difficult). What exactly is being proposed? The developer has an opportunity, if storm sewer invert elevations cooperate, to utilize the northeast corner of the property for proper non-structural BMP's.

No indication is provided for how the sidewalk along Detroit coordinates and ties into the existing sidewalk at the eastern terminus of the project limits. Additionally, there seems to be no delineated crossing location between the sidewalk along Detroit and the proposed entrance driveway.

Please confirm that if this project moves forward the developer will be incorporating a new pole (as shown in the master plan) for the wayfinding element fronting this property

The landscape plan (not the site plan) indicates a sidewalk connection behind the building to connect to Manor Park, which is commendable. Although, there is no indication of grades anywhere in the documents. Can a sidewalk connection be achieved given the difference in grades without steps or ramps?

A 3" autumn blaze maple (nice tree!) is planned for the northwest corner of the property, nearly on the property line behind what is assumed to be a retaining wall. The applicant stated that he was installing a versalock wall during the December ABR meeting. You are not able to plant a tree within the reinforcing zone of the top side of a versalock wall. The tree will not work where shown.

The applicant is showing a retaining wall to be built on the northern property line. This plan is not practical without a work easement from the neighbor. Versalock walls require a stone leveling pad below grade which is typically a few feet wider than the wall. Building the leveling pad would require access to the neighbor's property.

Serious consideration needs to be given to ADA compliance. During the last meeting a response was provided that ADA is the designers responsibility, but several issues, in addition to grade and slope (which are not addressed) I believe need addressing at this stage. Assuming that the rear entrances to these units will be utilized by customers (because that is where the parking is), the door swing direction should be checked. It is likely that doors may need to swing outward (not inward as shown) along the back of the building, which due to the narrow sidewalk will create issues with ADA clearance. Additionally, the location of the single ADA parking space is not convenient to the most number of businesses and could be challenged in the future. I have gone through this exact issue in the past.

~~There is no pedestrian circulation on the east side of the building.~~

~~The plans indicate upon completion and before maturation of the arborvitae that they will already encroach on the east driveway.~~

Please note that no comments on the building architecture are being provided. The Board seems to be doing the best they can with a design that leaves much to be desired.

Please accept my comments understanding that I only want the best for Lakewood and believe that the commercial corridors deserve and can command first class design, which this project obviously does not provide.

Sean McDermott

### Proposed Development Questions:

**Traffic safety:** Has the city conducted any traffic studies on the intersection to ensure it can accommodate the increased traffic without risking pedestrian safety?

**Egress:** Will traffic be required to turn left when exiting onto Manor Park to avoid an increase in traffic down the street? With pizza delivery as a part of this project, the traffic will be constant and create additional dangers to children and older pedestrians. Considering the traffic increases from Bob Evans and this project, will the state be approached about reevaluating the intersection for a traffic light?

**Access to Manor Park:** The cost and coordination needed to move the AT&T U-Verse box is immense. Can a project of this size justify the cost and effort to move the boxes? Have you approached AT&T and received a written commitment to move the boxes? If not, what is the secondary egress plan since the current proposal goes directly where the boxes are located?

**Proximity to neighboring apartment buildings and safety for those residents in regards to traffic -** The proposed driveways are feet away from these current residence buildings. Would you be able to provide the city code on the amount of space required?

**Tree removal:** The proposed drive to Manor Park will also remove a large tree. Will there be a commitment to replant more substantial shade trees along Manor Park?

**Pedestrian Circulation:** The commercial design guidelines call for pedestrian circulation. The building does a good job of fronting Detroit, but there is no pedestrian circulation on the east side of the building or from the rear of the building to Manor Park.

**Rear Access of Businesses:** Do the rear entrances to the buildings provide public access to the space? If public use is only through the front door and the majority of the parking is in the rear, pedestrians will need to use the narrow driveway to access the retail units, as will employees.

**Parking Count:** It appears the proposal may be short of the necessary parking required by code. Can you supply a parking calculation? If parking is indeed short, will a variance be applied for? Where will employees park?

**Grades and Slopes:** Grades and slopes are of concern and there is no indication of grades and slopes on the plans. How steep is the driveway? How will storm water be handled that is directed towards Manor Park? Doesn't the property need to drain toward Detroit? Will the site be built up? The proposal indicates that they will put in the piping to ensure water does not go downhill onto Manor Park, but this seems like a large undertaking. Would you be able to explain the specifics of how this will be done?

Tenants: Are the size and layout of the 6 units set? What businesses are they looking to attract to the development? Will tenants be secured before construction begins? Does the city require proof of financial security, both in terms of project completion and the ability to hold and maintain the property if spaces go unoccupied for an extended period of time?

September 8, 2014

Brenda Pongracz  
1377 Manor Park Avenue  
Lakewood, OH 44107  
216-221-8931  
[brendawepfer@hotmail.com](mailto:brendawepfer@hotmail.com)

Board of Building Standards  
12650 Detroit Avenue  
Lakewood, OH 44107  
[Planning@lakewoodoh.net](mailto:Planning@lakewoodoh.net)

Dear Board of Building Standards Members –

I have received information regarding the proposal from Daniel E. Margulies, Daniel Margulies Co. Inc., regarding the properties located at 14200 and 14206 Detroit Avenue, Docket No. 09-88-14. While I plan to attend the public meeting this Thursday, September 11, 2014 at 5:30pm, I have numerous concerns with this proposal and would like to ensure that they are all noted with the Board and addressed before this proposal is considered. After reviewing the plans as submitted, I do not feel that the proposal fits into the area as described and I am very concerned about the noise, traffic patterns, and lack of tenants for the property.

Before listing my concerns, I would like to state that I am in favor of new tenants using this empty property on Detroit. However, I do not feel that expanding the scope of the existing usage is a good idea considering the numerous apartments buildings and residences surrounding the property. We accept that there will be noise from Detroit Avenue and surrounding businesses. However, we do not wish to have this noise brought within inches of our driveway and property by placing a parking lot adjacent to our house. If this proposal were scaled back significantly to only include a few units and using the current parking available on the lot, I believe the residents of Manor Park Avenue would be supportive.

In regards to the proposal from Daniel E. Margulies, our primary concern is the proposed parking and traffic egress onto Manor Park. The proposed parking lot would come in direct contact with our property at 1377 Manor Park and would create noise and safety hazards for our driveway. In regards to this, I would like the following questions answered:

- Will our existing fence and driveway be protected and maintained? Will the fence need to be enlarged?
- How will the proposed parking lot be lit and will efforts be made to reduce noise and light pollution from affecting the surrounding properties?
- What precautions will be put into place to prevent cars from sliding down the hill in winter and crashing through our fence that protects our driveway and property? We have had many instances of cars and snow plows running into the fence at the back of our property that abuts the Giant Eagle parking lot. We do not wish to have the same issues so close to our house, garage, and driveway.

- What noise control measure will be put into place for the residents of the adjacent apartment buildings?
- Where will a dumpster be placed and how will garbage removal be handled? This cannot occur against our fence.
- As Manor Park Avenue has a downward slope, there has been a history of flooding and water issues for properties as you move down the hill. With the removal of the trees and plants from the hill of this property, how will water control be handled to prevent flooding on our property?

In reviewing the proposal, even with a six-space parking lot behind the structure, I do not see how there is ample parking for customers. The few spaces available on Manor Park Avenue will be removed due to the proposed driveway egress. Parking on Manor Park Avenue is zoned residential only from 8:00am - 4:00pm. The limited parking on Detroit Avenue is used by apartment building residents as well as the Winking Lizard and Giant Eagle. In short, there is not enough parking anywhere near this proposed shopping facility to accommodate patrons. Therefore, I would like a better understanding of the following issues:

- With only 6 parking spaces and 7 proposed storefronts, where will store employees park?
- The Lutheran Church on Detroit Avenue hosts numerous events throughout the week, using most of the street parking on Manor Park Avenue and creating significant traffic issues. Has this been considered by the developer? How will this issue be addressed on weeknights and weekends when the church is in use?
- How will the developers ensure that patrons do not exit using the existing parking lot and driveway for the neighboring apartment buildings?

The intersection of Manor Park Avenue and Detroit Avenue have a long history of traffic problems, especially since the removal of the traffic light. Numerous residents from the Barton Center and Westerly Apartments attempt to cross this intersection on a daily basis. In addition, school children and families use this intersection on a regular basis. In light of this, I would like the following questions addressed.

- How will traffic patterns be addressed to ensure traffic exiting onto Manor Park Avenue from the proposed driveway egress do not turn right, increasing traffic on the residential street? This will create an additional blind corner, with the proposed shopping center on the south of the driveway and the AT&T boxes and a large apartment building to the north.
- The speed limit on Manor Park Avenue is only 25 miles per hour and numerous small children live on the street. How will the city ensure the safety of residents and traffic compliance from the pizza delivery drivers from Dominos?

Finally, there are numerous empty storefront a few blocks down on Detroit, near Giel Avenue, that would be better suited to this type of development. Most chain pizza establishments do not last long in Lakewood due to Angelos and other local pizza restaurants. It does not appear that the developer has tenants already in place for this shopping center. We do not need additional cell phone stores or convenience stores in Lakewood, especially on this area of Detroit. What will occur if the Domino's pizza is not successful?

I would like to thank you for your time and attention to my concerns regarding this proposal. I am hopeful that the city can work with the developer and residents of Manor Park Avenue to create a structure suitable to the neighborhood and surrounding area. I do not believe, however, that the proposal as submitted should be allowed to move forward.

Sincerely,

Brenda Pongracz  
Resident

## **PEDESTRIAN HYBRID BEACON – DETROIT/MANOR PARK**

### **City of Lakewood**

September, 2014

The City of Lakewood is considering the possibility of a Pedestrian Hybrid Beacon (PHB) installation at the intersection of Detroit Avenue and Manor Park Avenue in Lakewood, Ohio. The following information has been compiled and is presented as part of the PHB warrant analysis:

1. 2014 NOACA Safety Review Summary
2. 2014 Traffic Count Data
3. OMUTGD Hybrid Beacon Warrant Analysis
4. 2013 Pedestrian Accident Report

### **Background**

The Detroit Avenue/Manor Park intersection was part of Lakewood's City Wide Signal Upgrade Project Phase 2. The following is a history of the intersection up to present day:

1. The intersection evaluated for CMAQ eligibility for PID 82794, Lakewood Phase II signals.
2. The City of Lakewood warrant analysis showed the intersection did not meet vehicular or pedestrian warrants; peak hour pedestrian volume 54 pedestrians, well under Warrant 4 minimum for any major street volume (107 pedestrians/hour.) The City was advised to complete removal analysis to determine if the traffic signal could be justified for retention at 100% local cost.
3. 9/22/2009: Mayor Ed Fitzgerald of Lakewood provided written notice to ODOT District 12 that City recommends removal of the traffic signal at Manor Park/Blossom Park.
4. 1/13/2012: Dennis Kucinich- Congressman sent a letter to Governor Kasich concerning the traffic signal removal at Manor Park.
5. 2/7/2012: Myron Pakush – ODOT District 12 Deputy Director attends meeting with Mayor Summers and Joe Beno of Lakewood to discuss Congressman Kucinich's concerns; ODOT and the City agree to incorporate additional pedestrian crossing warning signs.
6. The City requests ODOT Construction Department to add 2 solar-powered rectangular rapid flashing beacons on the Detroit approaches to Manor Park (Change Order 13, Project 10-0464.) Beacons were added at project cost at a cost of \$10,010.46.

7. A pedestrian was hit by a car while attempting to cross Detroit Avenue at Manor Park on August 28, 2013.
8. In early 2014 NOACA conducted a Safety Review of the intersection. The safety review revealed low "vehicle yield rates" even with the Rectangular Rapid Flashing Beacons.
9. A preliminary PHB Warrant was conducted in mid-2014 using the existing October 9, 2008 traffic counts. The count showed that 35 pedestrians cross Detroit Avenue at Manor Park and 1,015 vehicles were eastbound/westbound on Detroit during the PM peak hour (3:00 PM -- 4:00 PM). The length of the crosswalk at this location is approximately 58 feet. This data satisfied the "Guidelines for the Installation of Pedestrian Hybrid Beacons on Low Speed Roadways" (2012 OMUTCD Figure 4F-1).
10. Due to the age of the 2008 traffic count an updated traffic count was performed at Detroit/Manor Park on August 6, 2014. This count showed that 20 pedestrians cross Detroit Avenue at Manor Park and 897 vehicles were eastbound/westbound on Detroit during the PM peak hour (3:45 PM -- 4:45 PM). The length of the crosswalk at this location is approximately 58 feet. This data satisfies the "Guidelines for the Installation of Pedestrian Hybrid Beacons on Low Speed Roadways" (2012 OMUTCD Figure 4F-1).

### PHB Warrant Analysis

As part of the OMUTCD PHB Warrant Analysis a gap analysis was performed at the intersection of Detroit Avenue and Manor Park to see if there were enough available gaps for pedestrians to safely cross Detroit Avenue without the use of a crossing device.

The traffic count data was collected on August 6, 2014 from 3:45 PM – 4:45 PM at the intersection of Detroit and Manor Park.

#### Calculations:

- Crossing of 58 feet (curb ramp to curb ramp).
- Crossing speed of 3.5 ft/sec was used due to the types of pedestrians observed (including older pedestrians).

TEM: (Data used: 3:45 PM crossing at Detroit/Manor Park)

$$t = 58' / 3.5 \text{ ft/sec} = 16.6 \text{ seconds}$$

$$V \times e^{(-V \times t / T)} = \text{Number of gaps/hour that will be } \geq t$$

where:  $V = 897$  vehicles in 60 minutes (20 pedestrians crossed Detroit during this period of time)

T = 3600 seconds

Therefore, # of gaps/hour  $\Rightarrow$   $t = 14.3$ , which is  $< 16.6$

There are not enough available gaps to accommodate pedestrians.

#### OMUTCD – Section 4F – Pedestrian Hybrid Beacons

The decision to install a Pedestrian Hybrid Beacon (PHB) under the PHB Warrant at the intersection of Detroit Avenue and Manor Park Avenue was based on the following criteria:

- The proposed installation meets the OMUTCD required 100' minimum distance from an adjacent side street (Manor Park Avenue).
- The closest adjacent intersection to Manor Park is Olivewood Avenue; it is located approximately 260' west of Manor Park. Detroit/Olivewood is a t-intersection with the Olivewood approach to the south. It is accessible by a two-way left-turn lane along Detroit Avenue.
- The proposed location for the PHB has ample space for the installation. A traffic signal and Rapid Flashing Beacons pedestrian crossing Installation were previously installed at this intersection. For this reason there are no RW or utility restrictions at this location.
- ADA compliant curb ramps can physically be installed at this location.
- There are no signalized pedestrian crossings within 400' of this location.
- A median island and/or curb bump-outs would not be feasible at this location due to physical constraints.
- There was a pedestrian/vehicle accident at this location in August, 2013. The pedestrian was hit while attempting to cross Detroit at Manor Park.
- This intersection is in close proximity to various types of pedestrian/vehicle traffic generators. There is a bank on the southeast corner of the intersection, a church on the northwest corner of the intersection, and a large corporate building on the southwest corner of the intersection. The Westerly Senior Apartments and Barton Senior Center are located approximately 300' west of the intersection. A retail plaza is located approximately 200' west of the intersection. There are bus stops along Detroit Avenue and pedestrians cross the street at various locations.
- According to the August, 2014 traffic counts, 20 pedestrians crossed Detroit at Manor Park between 3:45 PM and 4:45 PM. The number of pedestrians crossing the road at this location is much higher during periods of time when schools are in session due to the large number of students. The traffic count also did not take into account the large

number of pedestrians that cross Detroit just west of Manor Park at the Westerly Senior Apartments.

- The location on Detroit Avenue west of Manor Park will provide the required stopping sight distance for drivers.
- The PHB installation may include the removal/relocation of on-street parking and bus stops depending upon final location.
- The 2014 Average Daily Traffic on Detroit at this location is approximately 11,700 vehicles per day.
- The posted speed on Detroit Avenue 25 miles per hour.
- The crossing length of Detroit Avenue in the vicinity of the intersection varies from 50 feet to 60 feet.

The pedestrians that cross Detroit Avenue are comprised of students (nearby schools), shoppers (nearby retail), and older adults (senior apartments/center). There are no other signalized crossings in the vicinity for these pedestrians to utilize. For this reason, it is apparent that a PHB installation would be utilized at this location and would provide a safer crossing environment for pedestrians.

According to the OMUTCD, the intersection of Detroit Avenue and Manor Park Avenue warrants a Pedestrian Hybrid Beacon installation.

### Manor Park Avenue/Detroit Avenue Safety Review

At the request of the Mayor, NOACA staff conducted a safety review of the intersection of Detroit Avenue and Manor Park Avenue in the City of Lakewood. Residents have expressed concern over pedestrian safety at the intersection. However, when examining crash data from 2008-2012 there were no documented pedestrian crashes.

A full signal was recently removed from the intersection based on a system-wide signal warrant analysis. The intersection still sees a relatively high amount of pedestrian crossings due to surrounding land uses and is projected to increase. To address safety concerns, the City installed a rectangular rapid flashing beacon (RRFB) to assist pedestrians in crossing. Despite this crossing treatment, the City has received feedback from residents that the current treatment does not allow for safe crossing.

On Wednesday, February 26, NOACA staff conducted a field review from 2-4 p.m. to observe the intersection, pedestrian and driver behavior, and experience the crossing as a pedestrian. NOACA staff was joined by the City's Director of Public Works, Joe Beno to provide some background information and discuss the RRFB installation and modification. Mayor Michael Summers also visited with staff to give the context of the resident's issues and history of the project. A summary of observations from the field review are:

- There is no crosswalk at the east approach of the intersection due to increased roadway width. Just west of this intersection, the roadway width changes from 3 to 5 lanes until the intersection of Burns Road and Detroit Avenue. The west approach crosswalk is approximately 58' in length.
- The intersection has a relatively higher proportion of at-risk pedestrians due to nearby senior homes and schools.
- Most pedestrians only cross Manor Park going east and west on Detroit. Less than a quarter appeared to be crossing Detroit.
- There do not appear to be sufficient gaps for pedestrians to cross without vehicles yielding with high volumes of cars and trucks consistently.
- The RRFB signage and pushbutton are installed on utility pole located approximately 15 ft. west of the corner and actual crosswalk.
- The northern pushbutton frequently failed to call the signal when pushed. This may have been due to the extremely cold temperatures.
- Vehicle yield rates appear to be very low as NOACA staff crossed the intersection multiple times wearing reflective clothing and observing other pedestrians.

NOACA staff commends the City of Lakewood for its initiative in addressing safety issues at this intersection. Additionally, the installation of the RRFB is a great step to

April 25, 2014

improve the function and safety for pedestrians crossing Detroit Ave. The following are some recommendations to improve not only this intersection, but pedestrian safety throughout the City.

Suggested Low-cost Countermeasures

- o Enforcement and/or educational programs (i.e. crosswalk enforcement actions)
- o Repair RRFB pushbutton on the north side of Detroit. (The day of the field review it was misfiring and not functioning consistently.)
- o Place RRFB signage and pushbutton closer to crosswalk on both sides of the intersection
- o High visibility crosswalks
- o Replace all pavement markings
- o Pedestrian warning signage (W11-2 & W16-9P<sup>1</sup>)
- o In-street pedestrian crossing sign (R1 6<sup>2</sup>)

Suggested Medium-cost Countermeasures

- o Curb extensions (particularly NW corner)
- o Lane diet with potential refuge island
- o Road diet (remove turn lanes)
- o Move crosswalk to mid-block location

These are just some general recommended countermeasures. Because the driver behavior and vehicle yield rates appeared very low during the field review, NOACA staff recommends further exploration of education and enforcement at this intersection and across the City with crosswalk enforcement actions. Moving the RRFB pushbuttons closer to the intersection and improving the visibility of the crosswalk would also be effective low-cost improvements. In the long-term, at this intersection and elsewhere on similar roadways, the City may want to explore deploying curb extensions to shorten pedestrian crossings, particularly where refuge islands cannot be deployed. At this particular location it did appear that there was ample right-of-way for a curb extension on the NW corner.

NOACA staff appreciates the opportunity to work with the City of Lakewood and will offer any assistance in implementing the recommended countermeasures. As a first step, we have already started exploring ways to develop and fund a crosswalk enforcement action initiative should the City express interest in participating.

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<sup>1</sup> Reference from *Manual on Uniform Traffic Control Devices for Streets and Highways*, 2009 Version

<sup>2</sup> Reference from *Manual on Uniform Traffic Control Devices for Streets and Highways*, 2009 Version

**NINE HOUR COUNT**

Date: Thursday, 11/09/2008

Time: 7-10 AM, 11 AM - 2 PM, 3 - 6 PM

Northbound and Southbound Approach: Major Park Avenue / Blossom Park Avenue

Eastbound and Westbound Approach: Detroit Avenue

Prepared By: RAW

Trucks: 3.2%

Buses: 1.7%

9 Hour to 24 Hour Multiplier: 1.71

Seasonal Adjustment Factor: 0.921

Intersection Peak Hour:

Start Time	Major Park SB APPROACH			Detroit WB APPROACH			Blossom Park NB APPROACH			Detroit EB APPROACH			TOTAL	HR TOTAL	
	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left			
7:00 AM	1	5	2	3	34	1	1	3	0	2	43	9	104	763	
7:15 AM	7	9	5	3	53	2	1	5	0	0	48	15	141	918	
7:30 AM	11	6	12	3	43	4	4	6	1	1	88	11	188	991	
7:45 AM	10	9	26	10	39	4	12	7	3	5	120	17	330	945	
8:00 AM	9	9	14	9	101	1	5	8	0	1	91	10	260	891	
8:15 AM	4	8	6	7	73	3	5	6	2	1	58	15	184	726	
8:30 AM	0	5	4	4	57	0	1	8	0	0	77	19	172	710	
8:45 AM	10	8	2	5	60	1	4	6	1	3	73	13	188	699	
9:00 AM	13	3	8	7	77	3	0	1	2	0	71	0	184	698	
9:15 AM	0	4	0	2	77	0	0	1	2	0	67	9	177		
9:30 AM	6	3	2	2	71	1	2	2	0	2	55	0	152		
9:45 AM	4	3	7	7	40	4	3	3	2	1	67	6	155		
10:00 AM															
10:15 AM															
10:30 AM															
10:45 AM															
11:00 AM	4	4	5	2	70	1	5	5	2	2	78	8	193	881	
11:15 AM	6	6	4	2	90	3	1	1	0	5	80	14	220	913	
11:30 AM	8	1	0	6	10	0	2	3	0	1	88	8	227	992	
11:45 AM	12	1	0	5	85	0	4	1	0	0	90	8	221	989	
12:00 PM	2	6	5	3	97	0	1	2	1	2	115	11	245	987	
12:15 PM	7	5	3	1	80	2	7	3	0	3	92	6	260	960	
12:30 PM	14	6	1	6	92	1	3	4	3	1	74	10	214	860	
12:45 PM	11	4	5	4	97	0	5	0	1	1	90	6	210	877	
1:00 PM	2	1	6	7	77	5	4	0	2	3	83	12	218	873	
1:15 PM	0	2	3	2	87	1	1	3	1	1	92	4	205		
1:30 PM	4	1	3	3	107	1	3	3	1	2	89	5	235		
1:45 PM	4	3	8	4	104	0	2	0	0	1	84	8	219		
2:00 PM															
2:15 PM															
2:30 PM															
2:45 PM															
3:00 PM	6	3	0	8	120	1	7	2	4	1	108	16	260	1130	
3:15 PM	12	10	7	0	127	3	1	0	5	1	140	10	297	1079	
3:30 PM	5	3	0	11	127	3	4	1	2	2	100	16	270	1046	
3:45 PM	8	3	7	11	110	2	2	2	1	2	100	17	260	1032	
4:00 PM	3	11	5	1	98	2	3	8	1	0	95	6	237	1018	
4:15 PM	7	7	0	11	131	1	7	4	1	0	107	12	264	1026	
4:30 PM	7	4	1	3	117	4	4	14	2	2	94	13	265	1060	
4:45 PM	0	12	5	4	104	1	1	5	1	2	93	14	250	1041	
5:00 PM	7	5	3	5	92	1	4	6	0	2	115	7	247	1095	
5:15 PM	8	4	0	12	100	0	0	3	0	0	133	11	288		
5:30 PM	5	4	5	7	121	0	5	7	0	2	87	13	256		
5:45 PM	11	13	0	11	124	2	4	7	1	4	100	12	304		
9 HR TOTAL	256	105	209	193	3241	58	122	153	42	56	3197	382	8094		
24 HR TOTAL	403	291	329	304	6101	91	192	241	66	88	5035	482	12747		

11/8

**Intersection Peak Hours**

AM Peak 7:30	34	20	59	24	314	12	24	26	0	8	365	59
PM Peak 3:00	34	18	26	36	430	0	14	10	12	6	410	68
MID Peak 11:1	27	14	21	10	377	3	0	10	1	8	387	41

**Corridor Peak Hours**

AM Peak 7:30	34	20	58	29	314	12	24	26	6	8	365	69
PM Peak 3:00	34	20	26	35	445	3	13	23	1	8	444	43
MID Peak 12:00	29	21	14	13	380	3	10	9	5	7	371	33

2/2  
2/19/09  
#12  
508  
5414  
10125

**NINE HOUR PEDESTRIAN COUNT**

Date: Thursday, 10/09/2008

Time: 7:00 AM, 11 AM - 2 PM, 3 - 6 PM

Method used: Southbound Approach: Major Park Avenue + Blossom Park Avenue  
Eastbound and Westbound Approach: Oxford Avenue

Start Time	South to North		East to West		South to North		West to East		Hourly Total Across District Ave.
	Total	Hourly Total	Total	Hourly Total	Total	Hourly Total	Total	Hourly Total	
6:00 AM									
6:15 AM									
6:30 AM									
6:45 AM									
7:00 AM	1	13	2	18	0	0	5	31	19
7:15 AM	3	13	3	20	1	9	3	38	
7:30 AM	3	13	7	23	1	8	0	35	
7:45 AM	0	11	7	23	4	11	14	35	
8:00 AM	1	3	3	19	3	5	13	20	11
8:15 AM	1	6	0	21	0	3	3	15	
8:30 AM	1	8	7	28	2	7	8	17	
8:45 AM	0	9	3	28	0	6	2	15	
9:00 AM	3	8	5	28	1	11	2	15	13
9:15 AM	2		13		4		6		
9:30 AM	1		7		3		6		
9:45 AM	2		1		0		0		
10:00 AM									
10:15 AM									
10:30 AM									
10:45 AM									
11:00 AM	2	8	7	30	1	13	4	16	21
11:15 AM	2	10	7	35	3	15	11	30	
11:30 AM	1	11	9	35	6	14	8	32	
11:45 AM	3	15	7	35	3	9	13	31	
12:00 PM	4	13	12	44	3	8	7	23	21
12:15 PM	3	13	7	48	2	8	1	20	
12:30 PM	5	10	9	50	1	8	7	31	
12:45 PM	1	8	10	52	2	9	6	28	
1:00 PM	4	13	14	51	3	11	9	27	24
1:15 PM	0		11		2		9		
1:30 PM	4		11		2		5		
1:45 PM	5		15		1		4		
2:00 PM									
2:15 PM									
2:30 PM									
2:45 PM									
3:00 PM	8	18	27	38	7	36	12	64	54
3:15 PM	2	14	39	41	15	33	15	63	
3:30 PM	6	20	28	74	10	24	12	63	
3:45 PM	5	10	19	64	4	17	25	69	
4:00 PM	3	10	12	62	8	17	11	54	33
4:15 PM	5	14	24	67	5	18	15	57	
4:30 PM	3	9	14	63	4	12	17	48	
4:45 PM	3	6	13	63	4	11	8	42	
5:00 PM	1	3	17	64	3	7	7	40	10
5:15 PM	0		19		1		14		
5:30 PM	1		14		3		13		
5:45 PM	1		14		8		12		
6:00 PM									
6:15 PM									
6:30 PM									
6:45 PM									
<b>9 HR TOT</b>	<b>83</b>		<b>414</b>		<b>114</b>		<b>398</b>		

54

35 PEAS





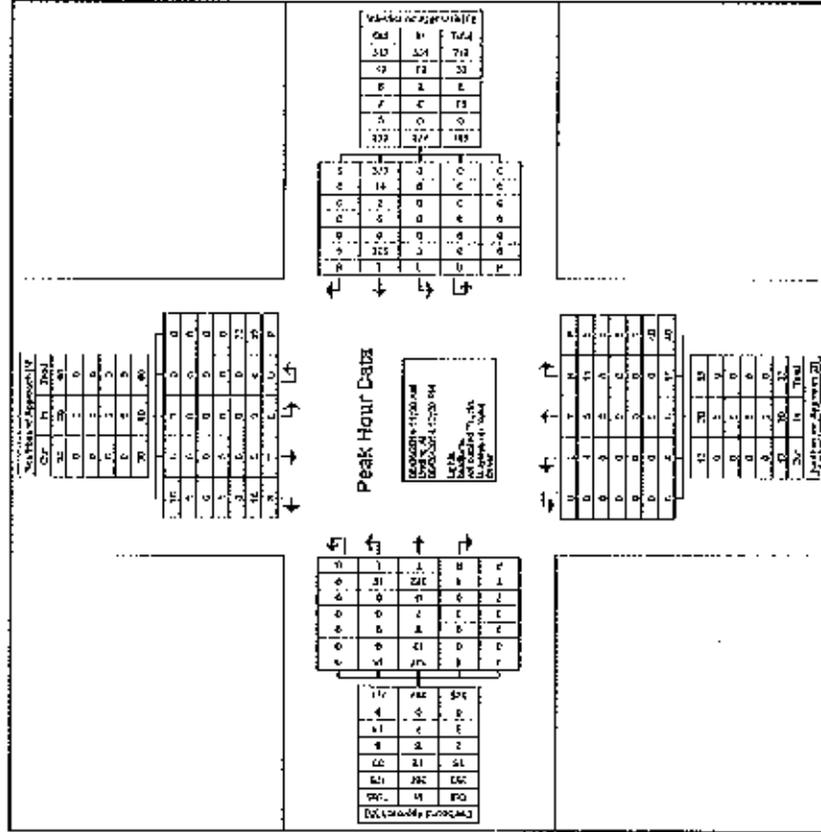






Ohio Department of Transportation  
 1960 West Broad Street  
 Mail Stop 5160  
 Columbus, Ohio, United States 43223  
 +1 614 752 389 Stephanie.McKillop@dota.state.oh.us  
 Office of Traffic Engineering

Court Name: US-3A @ Manor Park TMC  
 CC-DNED-174370  
 Site Code:  
 Start Date: 08/26/2014  
 Page No: 5



Turning Movement Peak Hour Data Plot (11:00 AM)







Ohio Department of Transportation

1980 West Street Street

Mail Stop 5160

Columbus, Ohio, United States 43223

+1-614-7526059 [Suparna.Mishra@dot.state.oh.us](mailto:Suparna.Mishra@dot.state.oh.us)

Office of Traffic Engineering

Count Name: US-SA @ Moner Paris TRAC

COLCNEJ - 174376

Site Code:

Start Date: 08/05/2014

Page No: 2



# TRAFFIC CRASH REPORT

Local Road Number: **100**      Case Number: **113005840**

Class Section: **7**      Unit of Case: **01**

1 - Fatal      2 - Injury      3 - PDO

1 - Severe      2 - Uninjured

From: **0102**      To: **0102**      State: **OH**      County: **0102**      City: **LAKWOOD**      Road: **LAKWOOD RD**

City: **LAKWOOD**      Road: **LAKWOOD RD**      Mileage: **0.2**      Direction: **01**

Driver License: **0102**      License Number: **0102**      License State: **OH**

Vehicle License: **0102**      License Number: **0102**      License State: **OH**

Police Agency: **0102**      Agency Name: **LAKWOOD PD**      Agency Address: **0102**

Officer Name: **0102**      Officer ID: **0102**

Location: **0102**      Location Name: **DETROIT AVE**      Location Type: **AV**

Direction: **0102**      Direction Name: **MAJOR PARK AVE**      Direction Type: **AV**

Reference Code: **0102**      Reference Code Name: **0102**

Reference Code: **0102**      Reference Code Name: **0102**

Vehicle Type: **0102**      Vehicle Type Name: **0102**

Vehicle Type: **0102**      Vehicle Type Name: **0102**

Vehicle Condition: **0102**      Vehicle Condition Name: **0102**

Vehicle Condition: **0102**      Vehicle Condition Name: **0102**

Vehicle Damage: **0102**      Vehicle Damage Name: **0102**

Vehicle Damage: **0102**      Vehicle Damage Name: **0102**

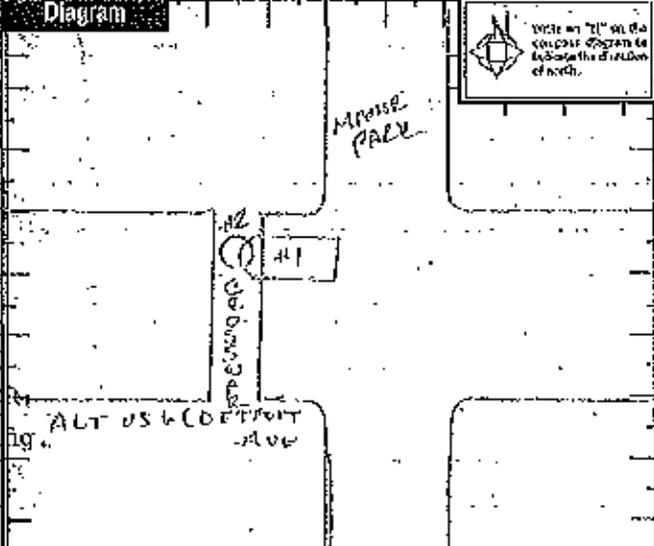
Vehicle Status: **0102**      Vehicle Status Name: **0102**

Vehicle Status: **0102**      Vehicle Status Name: **0102**

Vehicle Location: **0102**      Vehicle Location Name: **0102**

Vehicle Location: **0102**      Vehicle Location Name: **0102**

Unit#1 was traveling westbound on Alt US6 approaching Manor Park Ave. Unit#2 (pedestrian) was standing near the crosswalk. Unit#2 pushed the light and crossed the street. Unit#1 failed to yield at the crosswalk and entered the intersection. Unit#1 struck Unit#2. Unit#2 complained of right leg and hip pain. He was transported to Lakewood PD. He was treated and released with bruising.



Report Taken By: **0102**      Report Taken By Name: **0102**

Case Number: **0102**      Case Number Name: **0102**

Date: **0102**      Time: **0102**

Officer's Name: **0102**      Officer's Name: **0102**

Officer's ID: **0102**      Officer's ID: **0102**



# MOTORIST / NON-MOTORIST / OCCUPANT

Local Police Number: **1201051840**

DL Number: <b>01</b>	Name: Last, First, Middle: <b>CORRIGAN, TIM P.</b>	Date of Birth: <b>11/09/1960</b>	Age: <b>52</b>	Gender: <input checked="" type="checkbox"/> F - FEMALE <input type="checkbox"/> M - MALE
----------------------	--	----------------------------------	----------------	---

Address, City, State, Zip: <b>3192 WEST 142<sup>ND</sup> ST. CLEVELAND OHIO 44111</b>	County Police - INQUIRY AREA CODE: <b>(216) 252-7521</b>
---	--

INSURANCE: <input checked="" type="checkbox"/>	INSURED TAKEN BY: <input checked="" type="checkbox"/>	EMS Agency: _____	Medical Examiner Report Taken To: _____	Safety Equipment Used: <b>29</b>	DOT Compliance: <input type="checkbox"/> Mechanical <input type="checkbox"/> Electrical	SEARCHED: <input checked="" type="checkbox"/>	ALL BILLS PAID: <input checked="" type="checkbox"/>	EXPIRES: <input type="checkbox"/>	TRANSFERRED: <input type="checkbox"/>
--	---	-------------------	---	----------------------------------	--	---	---	-----------------------------------	---------------------------------------

DL State: <b>OH</b>	DRIVER LICENSE NUMBER: <b>RT 607223</b>	DL CLASS: <b>4</b>	HEARING: <input type="checkbox"/> NO <input type="checkbox"/> YES	RESTRICTIONS: <input type="checkbox"/>	ALCOHOL/DUI TESTED: <input type="checkbox"/>	ALCOHOL TEST SCORE: <b>0</b>	AUTISM TEST TYPE: <input type="checkbox"/>	AUTISM TEST VALUE: _____	DATE TEST SIGNATURE: <input type="checkbox"/>	DATE TEST VALID: <input type="checkbox"/>
---------------------	---	--------------------	--	--	--	------------------------------	--	--------------------------	---	---

Offense Code(s): <b>371.01A</b>	Offense Description: <b>FTY TO PEDESTRIAN</b>	Offense Number: <b>D 197603</b>	Issue Fee: <input type="checkbox"/>	Offense Dismissed By: <input type="checkbox"/>
---------------------------------	---	---------------------------------	-------------------------------------	--

DL Number: <b>02</b>	Name: Last, First, Middle: <b>STRAGISHER, GEORGE W.</b>	Date of Birth: <b>05/13/1934</b>	Age: <b>79</b>	Gender: <input checked="" type="checkbox"/> F - FEMALE <input type="checkbox"/> M - MALE
----------------------	---	----------------------------------	----------------	---

Address, City, State, Zip: <b>14306 DETROIT AVE # 826 LAKWOOD OHIO 44107</b>	County Police - INQUIRY AREA CODE: <b>(216) 221-0137</b>
--	--

INSURANCE: <input checked="" type="checkbox"/>	INSURED TAKEN BY: <input checked="" type="checkbox"/>	EMS Agency: <b>LAKWOOD SQUAD</b>	Medical Examiner Report Taken To: <b>LAKWOOD BR.</b>	Safety Equipment Used: <b>09</b>	DOT Compliance: <input type="checkbox"/> Mechanical <input type="checkbox"/> Electrical	SEARCHED: <input checked="" type="checkbox"/>	ALL BILLS PAID: <input checked="" type="checkbox"/>	EXPIRES: <input checked="" type="checkbox"/>	TRANSFERRED: <input type="checkbox"/>
--	---	----------------------------------	--	----------------------------------	--	---	---	--	---------------------------------------

DL State: _____	DRIVER LICENSE NUMBER: _____	DL CLASS: <input type="checkbox"/>	HEARING: <input type="checkbox"/> NO <input type="checkbox"/> YES	RESTRICTIONS: <input type="checkbox"/>	ALCOHOL/DUI TESTED: <input type="checkbox"/>	ALCOHOL TEST SCORE: <b>0</b>	AUTISM TEST TYPE: <input type="checkbox"/>	AUTISM TEST VALUE: _____	DATE TEST SIGNATURE: <input type="checkbox"/>	DATE TEST VALID: <input type="checkbox"/>
-----------------	------------------------------	------------------------------------	--	--	--	------------------------------	--	--------------------------	---	---

Offense Code(s): <b>(F) Local Code</b>	Offense Description: _____	Offense Number: _____	Issue Fee: <input type="checkbox"/>	Offense Dismissed By: <input type="checkbox"/>
--	----------------------------	-----------------------	-------------------------------------	--



DL Number: _____	Name: Last, First, Middle: _____	Date of Birth: _____	Age: _____	Gender: <input type="checkbox"/> F - FEMALE <input type="checkbox"/> M - MALE
------------------	----------------------------------	----------------------	------------	--

Address, City, State, Zip: _____	County Police - INQUIRY AREA CODE: _____
----------------------------------	--

INSURANCE: <input type="checkbox"/>	INSURED TAKEN BY: <input type="checkbox"/>	EMS Agency: _____	Medical Examiner Report Taken To: _____	Safety Equipment Used: _____	DOT Compliance: <input type="checkbox"/> Mechanical <input type="checkbox"/> Electrical	SEARCHED: <input type="checkbox"/>	ALL BILLS PAID: <input type="checkbox"/>	EXPIRES: <input type="checkbox"/>	TRANSFERRED: <input type="checkbox"/>
-------------------------------------	--	-------------------	---	------------------------------	--	------------------------------------	--	-----------------------------------	---------------------------------------

DL Number: _____	Name: Last, First, Middle: _____	Date of Birth: _____	Age: _____	Gender: <input type="checkbox"/> F - FEMALE <input type="checkbox"/> M - MALE
------------------	----------------------------------	----------------------	------------	--

Address, City, State, Zip: _____	County Police - INQUIRY AREA CODE: _____
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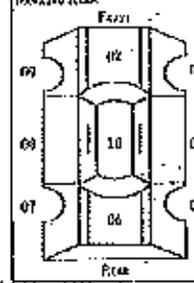
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LOCAL IDENT NUMBER  
1-600-584-1111

Unit Powers	Vehicle Hours Last Party Month	(1) Save As Default	Damage Event Number - INC. AREA CODE	(1) Save As Default
011				
Damage Address City, State, Zip (1) Save As Default				

LP Code	Vehicle Plate Number	VEHICLE IDENTIFICATION NUMBER	# OF OCCUPANTS
011	EA 84XN	ZEMRAERZK468118ZPB1	011
Vehicle Year	Vehicle Make	Vehicle Model	Vehicle Color
2000	FORD	FLEETSTL	BLUE
Year of Insurance Start	Insurance Company	Vehicle Number	Toner #
1	SAPCO	33PW074918	
Garage Name, Address, City, State, Zip			Georgia Profile - INCLUDE AREA CODE



US DOT	Vehicle Weight GVWR/GCW	Class Body Type	Trailer Description
	1 - Less Than 6,000 Lbs. to 10,000 Lbs. 2 - 10,000 to 26,000 Lbs. 3 - More Than 26,000 Lbs.	01 - No Other Class Information 02 - Bus/Box 40-55 Seats, No Driver 03 - Bus/Box 56+ Seats, No Driver 04 - Vehicle Towed Aboard Vehicle 05 - Lorry 06 - Limousine (Passenger Excess) 07 - Cais/Van/Coach/Bus 08 - Coach, Bus, Buggy 09 - Flat 10 - Cargo Tank 11 - Flat Bed 12 - Dump 13 - Cranes/Lifts 14 - Aerial Lifts/Booms 15 - Flatbed/Trailer 16 - Other/Unknown	1 - Two-Way, Not Limited 2 - Two-Way, Not Limited, Certain Low Speed Limit 3 - Two-Way, Limited, Maximum Gross Vehicle Weight Limited 4 - Two-Way, Limited, Maximum Gross Vehicle Weight Limited 5 - One-Way, Tractor Unit
HM Rating LP No.			
HM Class			

How Damaged (Location, Page to Damage)	Type of Use	How Tied	Placard	Has HM Placard
01 - Intentional - Major Damage 02 - Intentional - No Damage 03 - Intentional - Minor 04 - Intentional - Major Damage 05 - Vehicle Make - Other Location 06 - Other Location 07 - Structural Damage 08 - Structural 09 - Major Structural Damage 10 - Minor Structural 11 - Structural Damage 12 - Non-Structural Area 13 - Other/Unknown	1 - Personal 2 - Commercial 3 - Government  <input type="checkbox"/> In Emergency Response	01 - None 02 - Spill/Leak 03 - Collision 04 - Fuel Spill 05 - Minor 06 - Severe Damage 07 - Power 08 - Van 09 - Mechanical 10 - Mechanical 11 - Structural/AV 12 - Other/Unknown	01 - None 02 - Spill/Leak 03 - Collision 04 - Fuel Spill 05 - Minor 06 - Severe Damage 07 - Power 08 - Van 09 - Mechanical 10 - Mechanical 11 - Structural/AV 12 - Other/Unknown	<input type="checkbox"/>

Special Features	How Damaged Area	Vehicle Area	Other
01 - None 02 - None 03 - Radio 04 - Sun - Special Material 05 - Bus - Window 06 - Bus - Window 07 - Bus - Window 08 - Bus - Window	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	1 - None 2 - None 3 - None 4 - None 5 - None 6 - None 7 - None 8 - None 9 - None

Pre-Damage Actions	How Damaged Area	Vehicle Area	Other
01 - None 02 - None 03 - None 04 - None 05 - None 06 - None	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	1 - None 2 - None 3 - None 4 - None 5 - None 6 - None 7 - None 8 - None 9 - None

Continuing Circumstances	How Damaged Area	Vehicle Area	Other
01 - None 02 - None 03 - None 04 - None 05 - None 06 - None 07 - None 08 - None 09 - None 10 - None	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	1 - None 2 - None 3 - None 4 - None 5 - None 6 - None 7 - None 8 - None 9 - None

Sequence of Events	How Damaged Area	Vehicle Area	Other
1 - None 2 - None 3 - None 4 - None 5 - None 6 - None	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	01 - None 02 - Front 03 - Rear 04 - Side 05 - Top 06 - Bottom 07 - Wheel 08 - Tire 09 - Axle 10 - Suspension 11 - Brake 12 - Steering 13 - Drive 14 - Transmission 15 - Engine 16 - Exhaust 17 - Fuel System 18 - Electrical 19 - HVAC 20 - Other	1 - None 2 - None 3 - None 4 - None 5 - None 6 - None 7 - None 8 - None 9 - None

Unit Status	Police Status	Traffic Control	Unit Disposition
1 - None 2 - None 3 - None 4 - None	1 - None 2 - None 3 - None 4 - None	01 - No Control 02 - Stop Sign 03 - Vehicle Stop 04 - Traffic Sign 05 - Lane Closure 06 - Stop Sign 07 - Stop Sign 08 - Stop Sign 09 - Stop Sign 10 - Stop Sign 11 - Stop Sign 12 - Stop Sign	From 3 To 1 1 - None 2 - None 3 - None 4 - None 5 - None 6 - None 7 - None 8 - None 9 - None



**TRAFFIC ACCIDENT SUPPLEMENTARY REPORT**

REPORT NO. 13-005840

Driver's Name TIM P. COLLIGAN Phone (216) 252-7521

Address 3192 WEST 142<sup>ND</sup> ST. CLEVELAND OHIO 44111  
Street Number City State Zip Code

Place of Employment \_\_\_\_\_ Address \_\_\_\_\_ Phone \_\_\_\_\_

Date of Birth 11-9-60 S.S. No. \_\_\_\_\_ Driver's Lic. No. RT 607223 State OHIO

Vehicle License Number EA B4XN State OHIO Year 2013

Make of Car FORD Model FLEXSTAR Color BLUE  2d  4d  CVT Year 2009

Motor Cycle \_\_\_\_\_ (X) \_\_\_\_\_ Student \_\_\_\_\_ School \_\_\_\_\_

Accident Location DETROIT / BIRCH MANOR PARK Date & Time 8-28-13 1057

Owner's Name & Address SAME

Phone \_\_\_\_\_

Insurance Co. SAFECO Policy No. 33 P0676918

Injury  No  Yes Sex Male

"If your car has been towed you must present either your license registration or Certificate of Title at the Lakewood Police Dept., 12650 Detroit Avenue for a car release. **WARNING: IF YOUR AUTO IS NOT CLAIMED WITHIN 30 DAYS IT WILL BE DEEMED ABANDONED AND SUBJECT TO DISPOSAL ACCORDING TO LAW.**"  
 According to Ohio Revised Code Section 4509.06(A), "The driver of any motor vehicle which is in any manner involved in a motor vehicle accident within six months of the accident may forward a written report of the accident to the Registrar of Motor Vehicles on the form prescribed by the registrar alleging that a driver or owner of any other vehicle involved in the accident was uninsured at the time of the accident". Forms may be obtained at the police station.

Describe What Happened:

*EAST BOND ON DETROIT  
 AT MANOR PK I STOPPED FOR SIGN  
 MAN STOPPED AS I WAS GOING TO  
 YMCA HE JUMPED ONTO HOOD  
 AND FELL AFTER A "HOWLER"*

8-28-13  
 Date and Time of Statement

*Timothy P. Colligan*  
 Signature

**TRAFFIC ACCIDENT SUPPLEMENTARY REPORT**

REPORT NO. 13-005840

Driver's Name GEORGE W. STRAGISHER Phone (216) 221-0137

Address 14306 DETROIT AVE # 826 LAKWOOD OHIO 44107  
Street Number City State Zip Code

Place of Employment \_\_\_\_\_ Address \_\_\_\_\_ Phone \_\_\_\_\_

Date of Birth 5-13-84 S.S. No. \_\_\_\_\_ Driver's Lic. No. \_\_\_\_\_ State \_\_\_\_\_

Valid License Number \_\_\_\_\_ State \_\_\_\_\_ Year \_\_\_\_\_

Make of Car \_\_\_\_\_ Model \_\_\_\_\_ Color \_\_\_\_\_  2d  4d  CV  1/2k Year \_\_\_\_\_

Motor Cycle \_\_\_\_\_ CC \_\_\_\_\_ Student \_\_\_\_\_ School \_\_\_\_\_

Accident Location DETROIT / MAJOR PARK Date & Time 8-28-13 1057

Owner's Name & Address \_\_\_\_\_

Phone \_\_\_\_\_

Insurance Co. \_\_\_\_\_ Policy No. \_\_\_\_\_

Injury  No  Yes Seat Belt \_\_\_\_\_

"If your car has been towed you must present either your license registration or Certificate of Title at the Lakewood Police Dept., 12650 Detroit Avenue for a car release. **WARNING: IF YOUR AUTO IS NOT CLAIMED WITHIN 30 DAYS IT WILL BE DEEMED ABANDONED AND SUBJECT TO DISPOSAL ACCORDING TO LAW.**"  
 According to Ohio Revised Code Section 4502.06(a), "The driver of any motor vehicle which is in any manner involved in a motor vehicle accident within six months of the accident may forward a written report of the accident to the Registrar of Motor Vehicles on the form prescribed by the registrar alleging that a driver or owner of any other vehicle involved in the accident was uninsured at the time of the accident". Forms may be obtained at the police station.

**Describe What Happened:**

I WAS ON DETROIT AND MAJOR PARK WAITING TO CROSS THE STREET. I PRESSED THE LIGHT FOR THE CROSSWALK. THE LIGHTS STARTED FLASHING SO I STARTED CROSSING THE STREET. WHEN I WAS CROSSING A VAN STARTED SLOWING DOWN. I CONTINUED ACROSS THE STREET AND WAS HIT BY THE SAME VAN.

\_\_\_\_\_ Date and Time of Statement 8/28/13 \_\_\_\_\_ Signature George W. Stragisher

Figure 4F-1. Guidelines for the Installation of Pedestrian Hybrid Beacons on Low-Speed Roadways

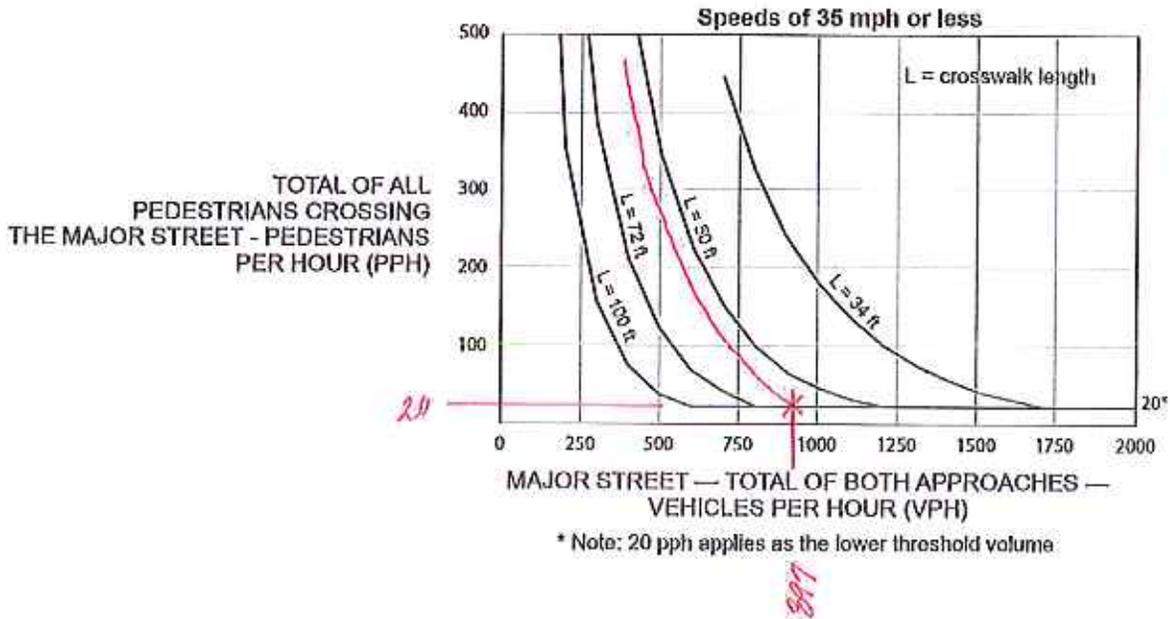
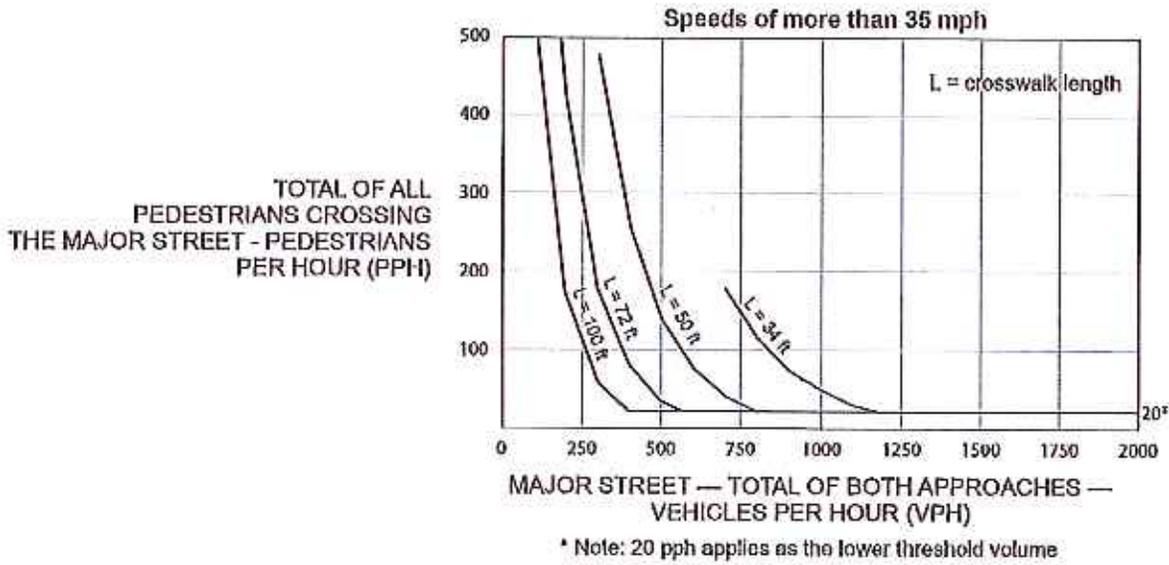
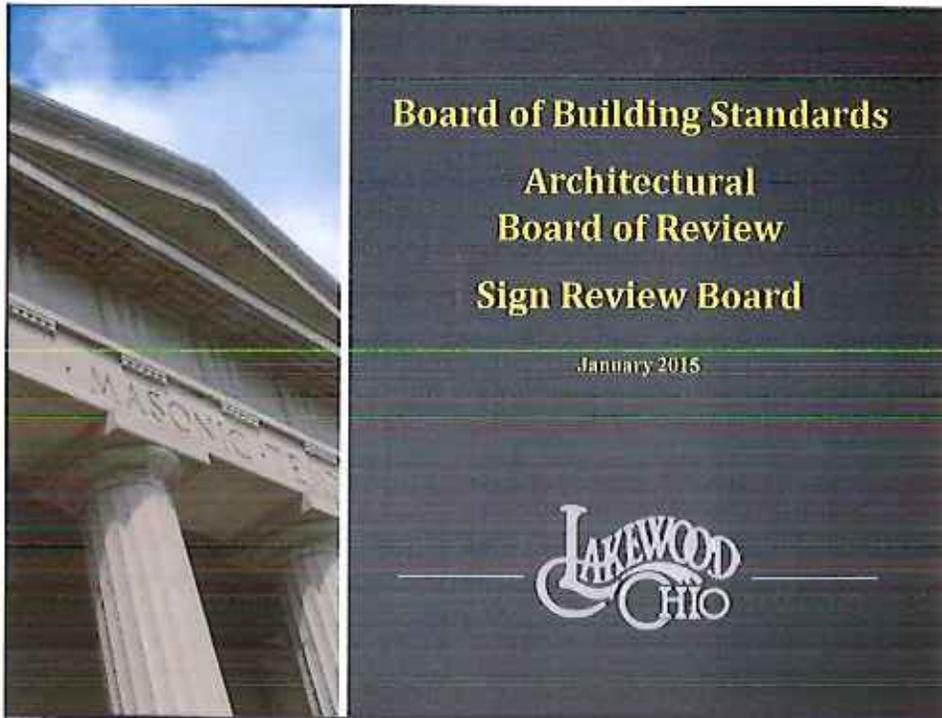


Figure 4F-2. Guidelines for the Installation of Pedestrian Hybrid Beacons on High-Speed Roadways





**Board of Building Standards**  
**Architectural**  
**Board of Review**  
**Sign Review Board**

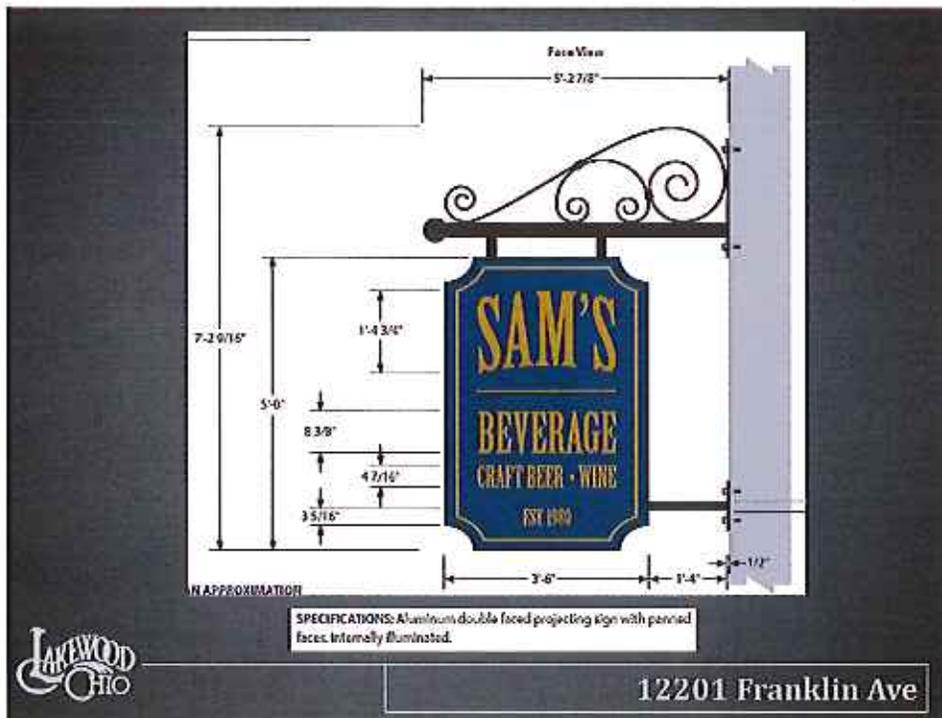
January 2015

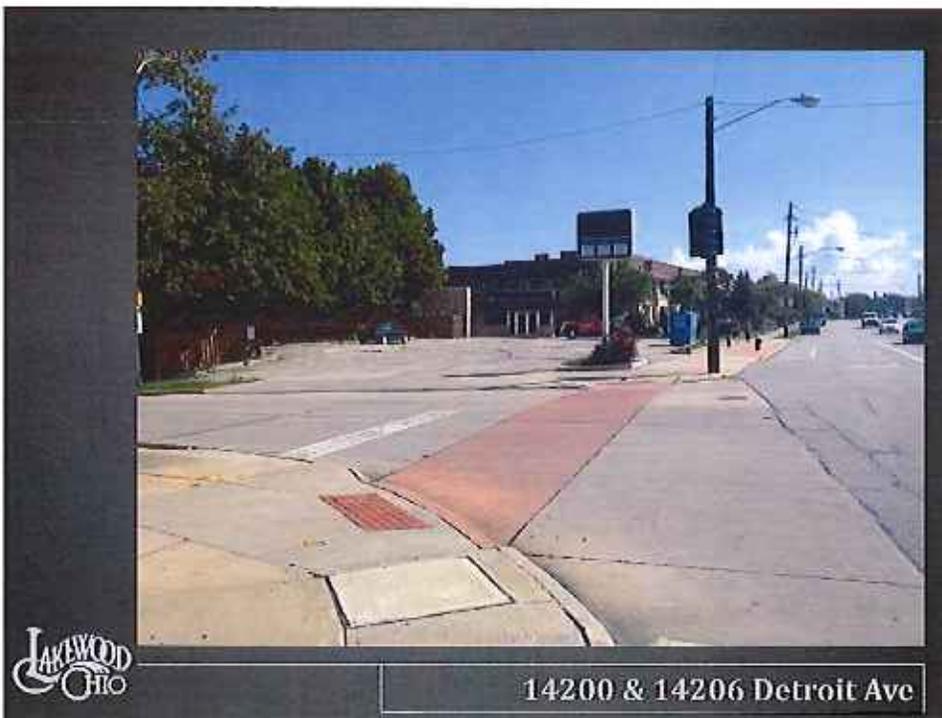
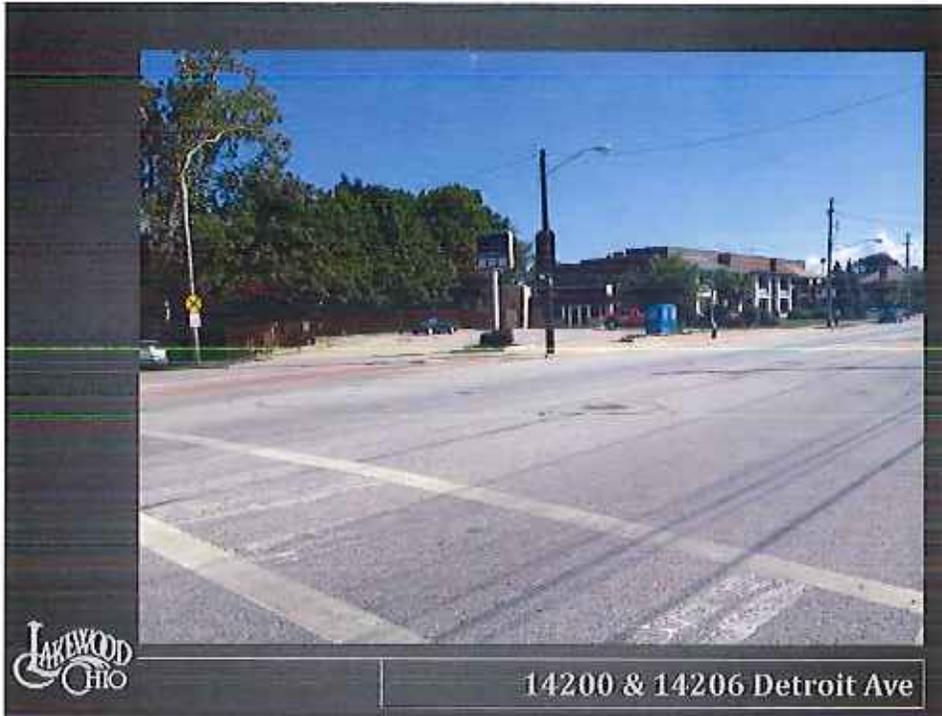
LAKESIDE  
OHIO



LAKESIDE  
OHIO

12201 Franklin







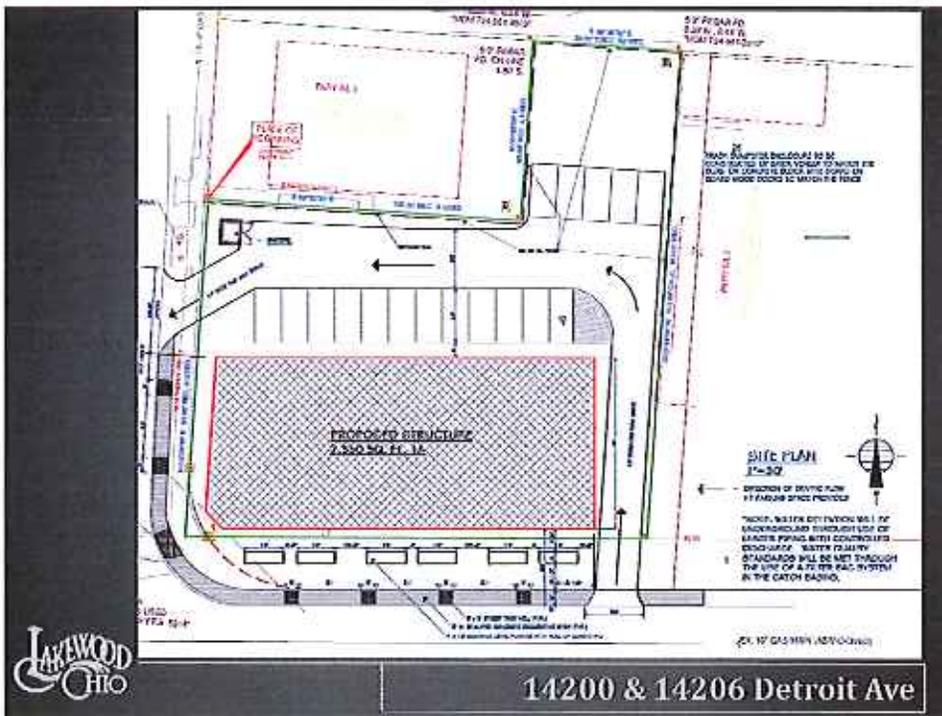
LAKELWOOD  
OHIO

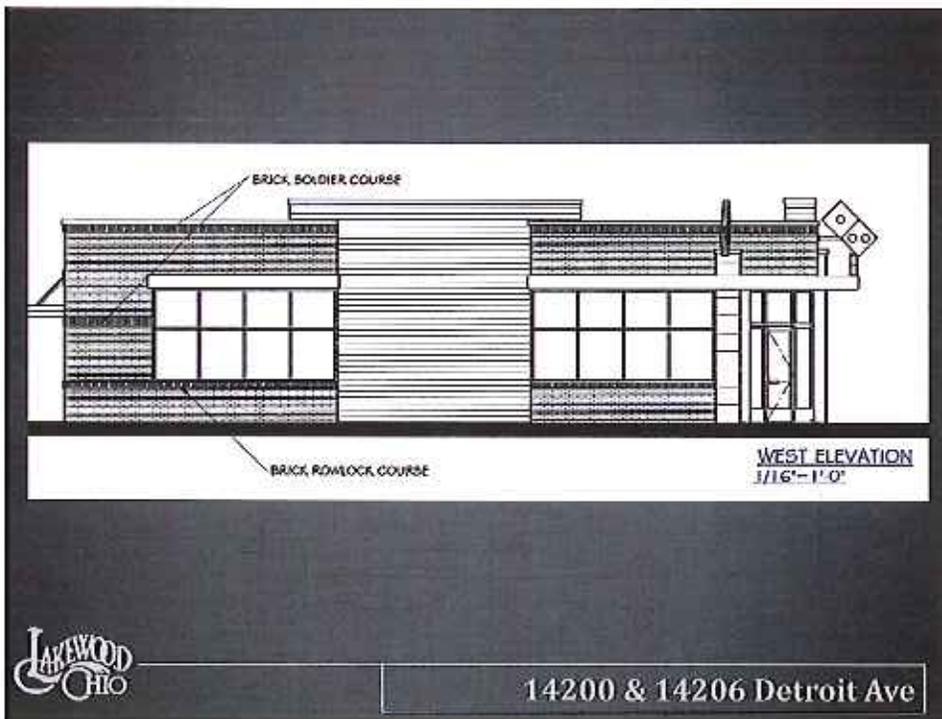
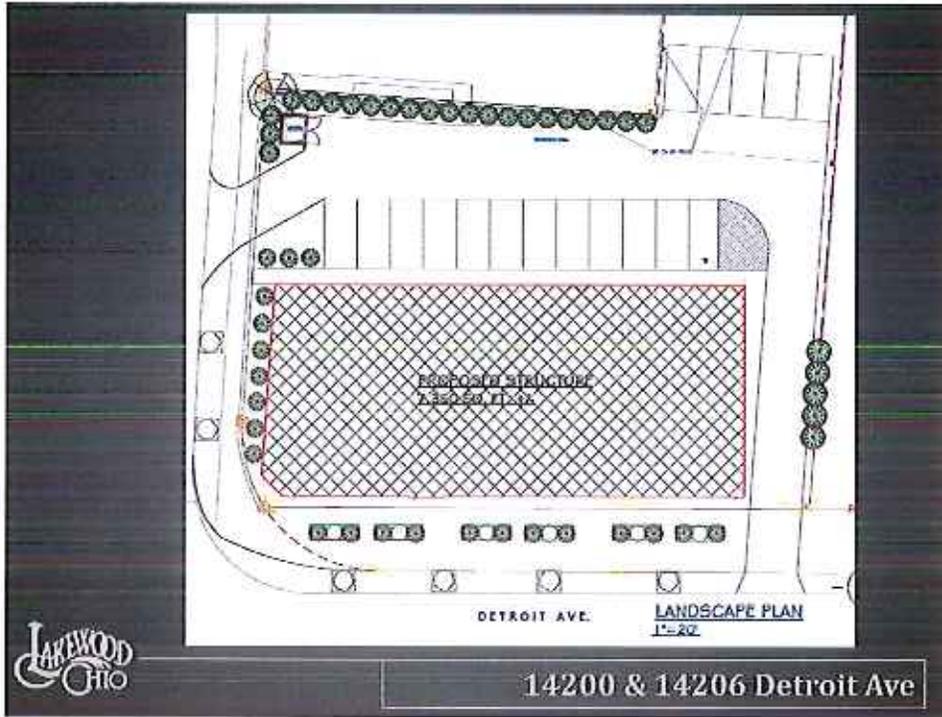
14200 & 14206 Detroit Ave



LAKELWOOD  
OHIO

14200 & 14206 Detroit Ave







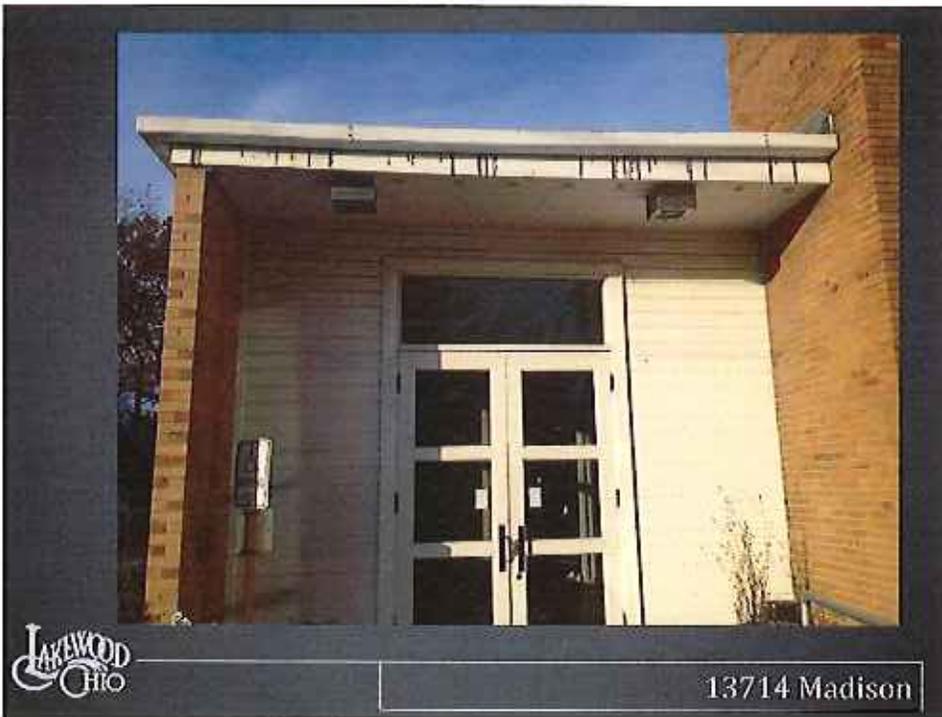
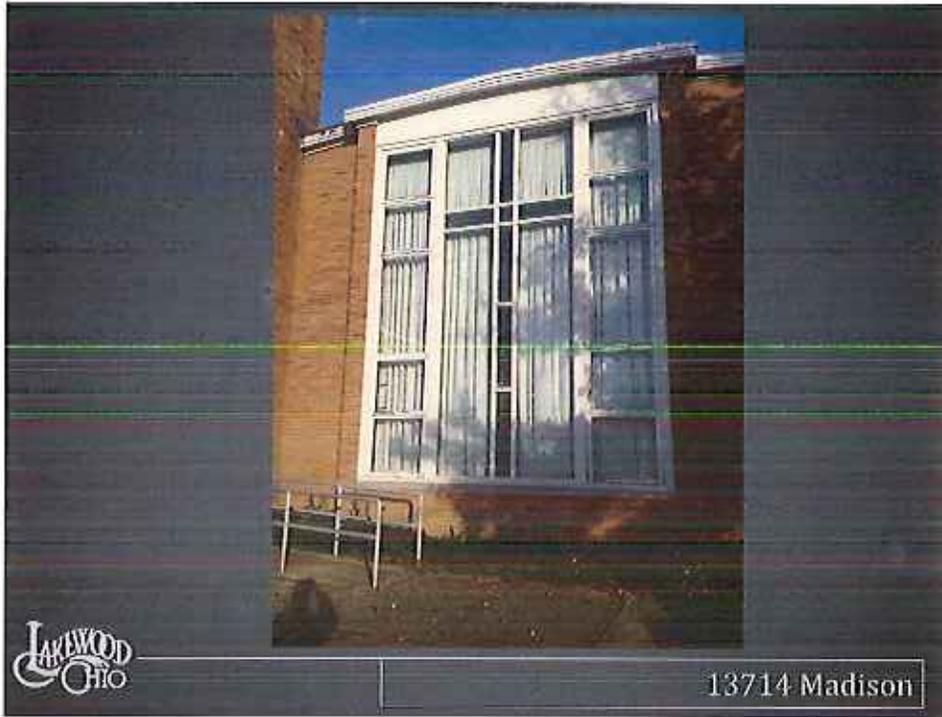


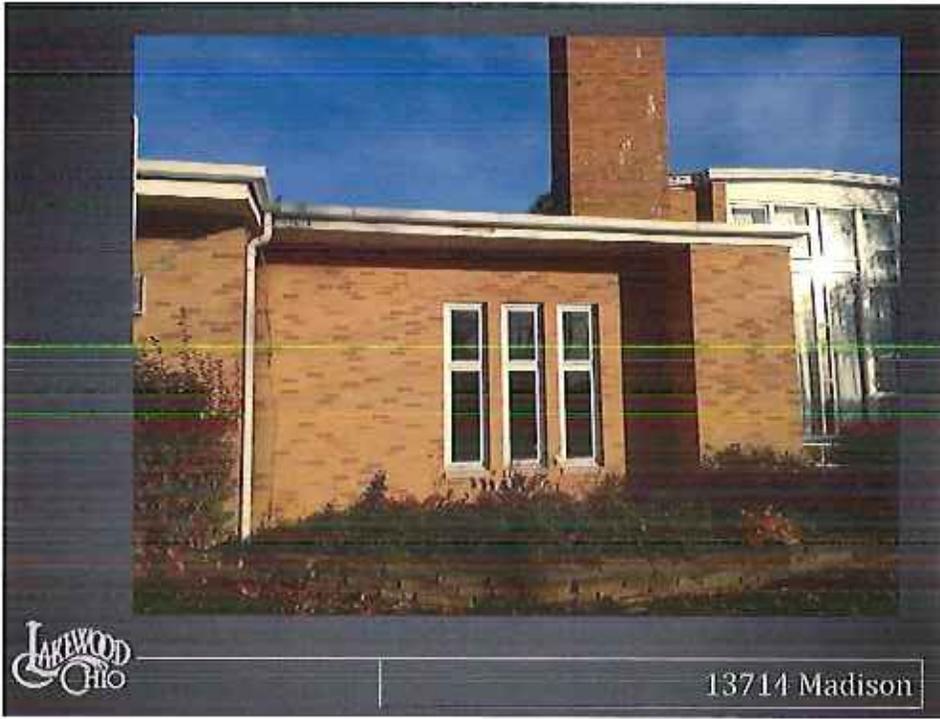


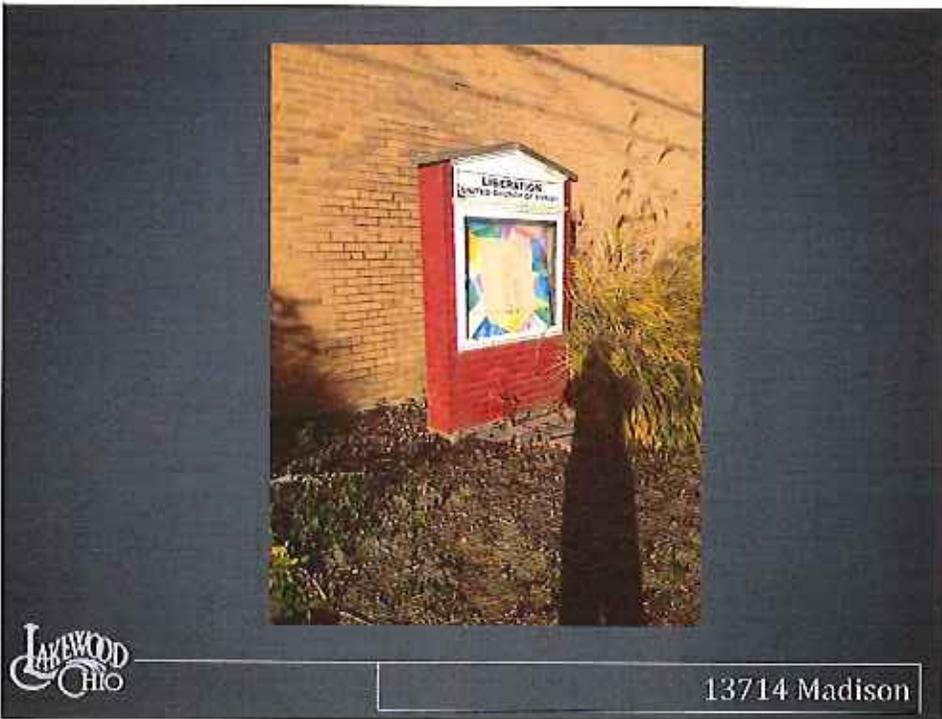
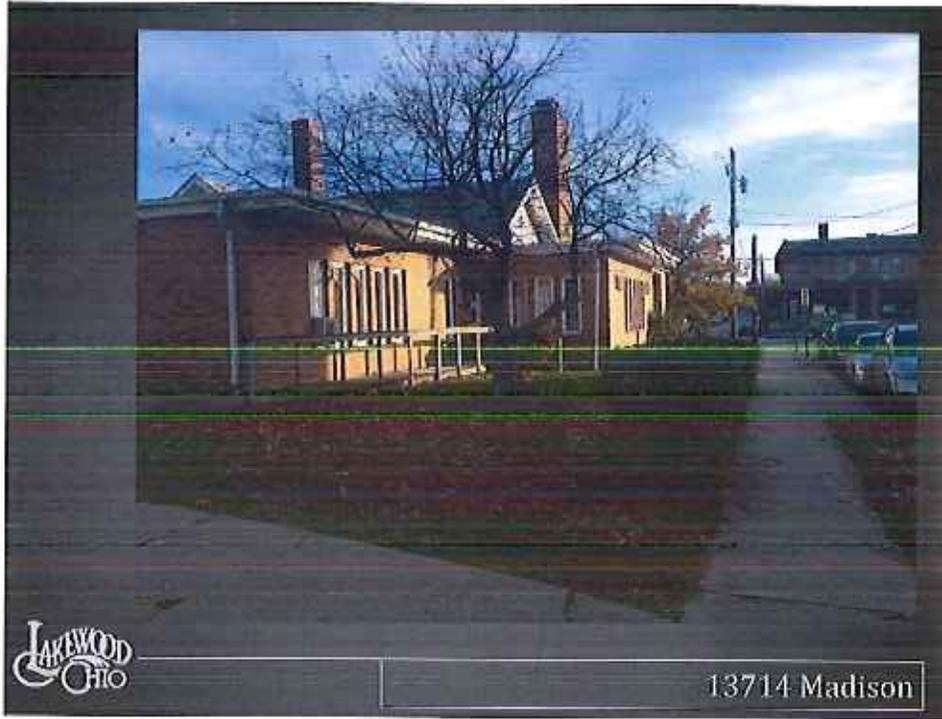


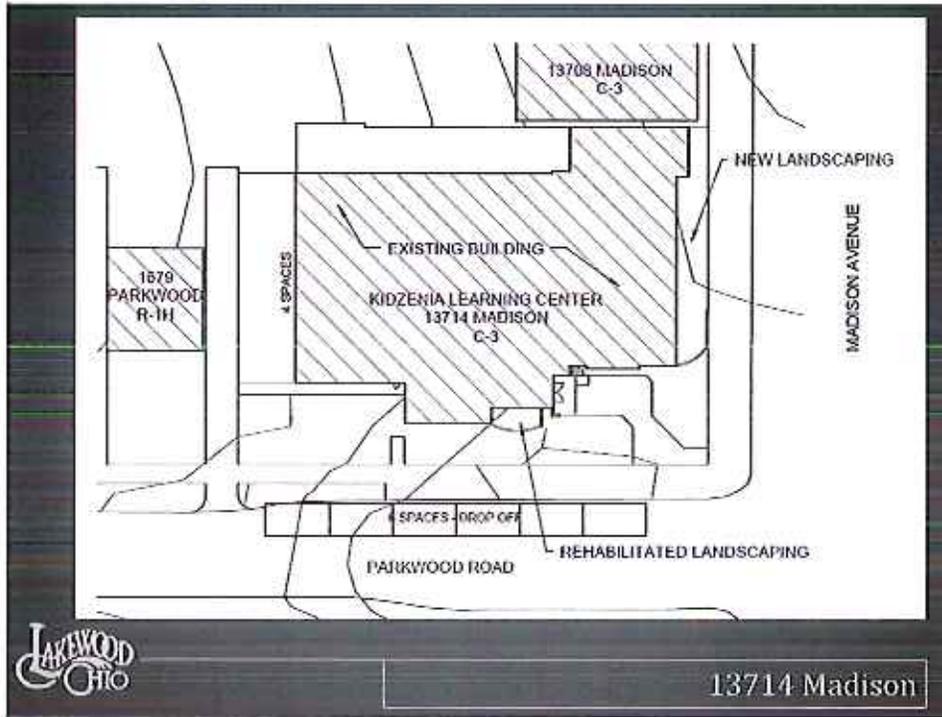


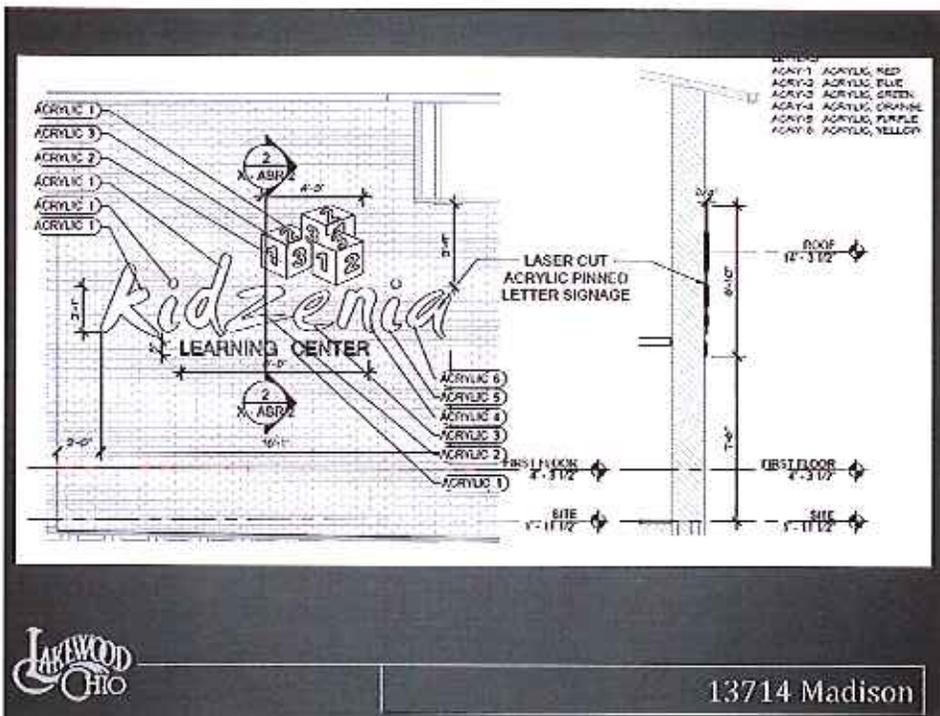




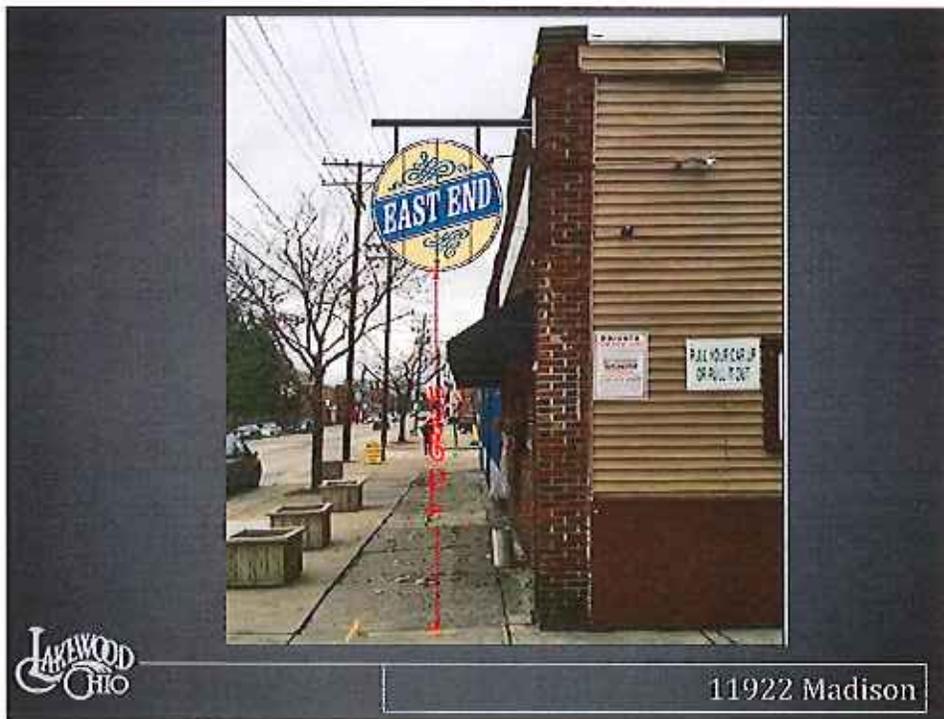
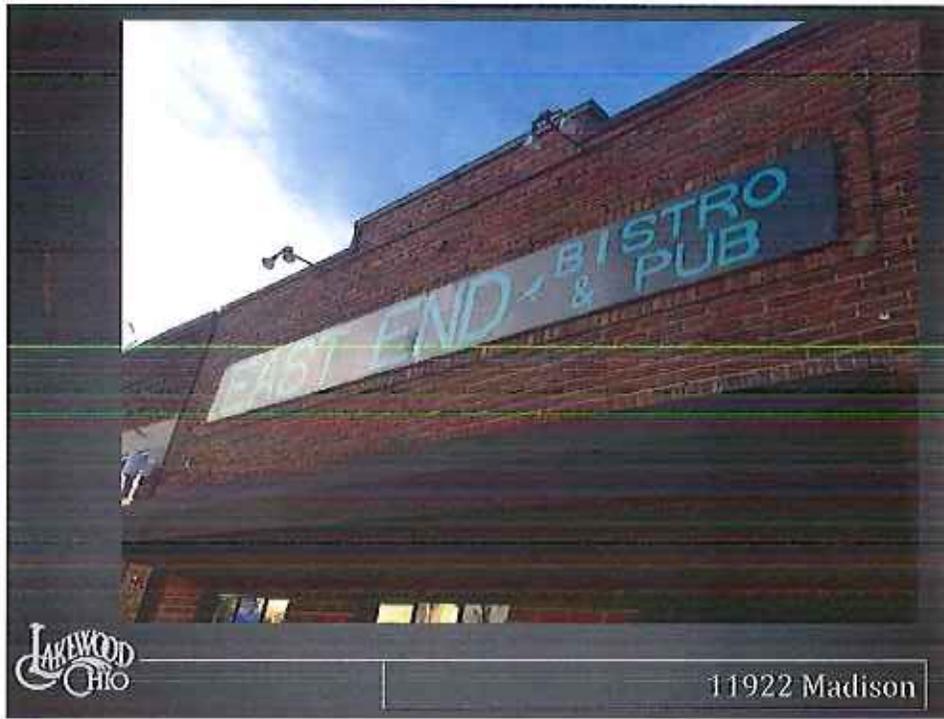


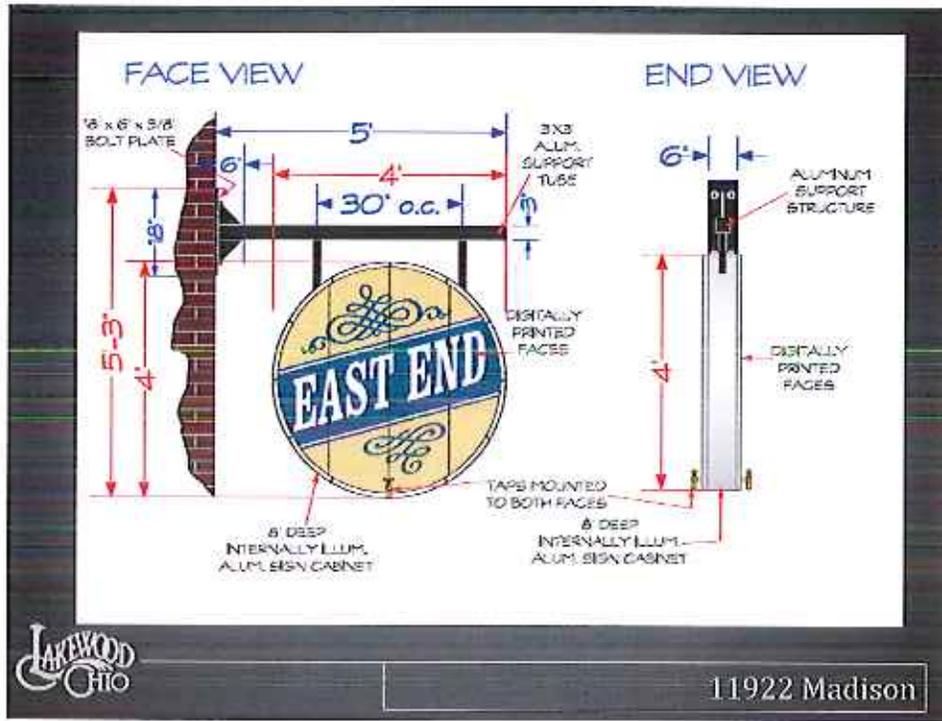






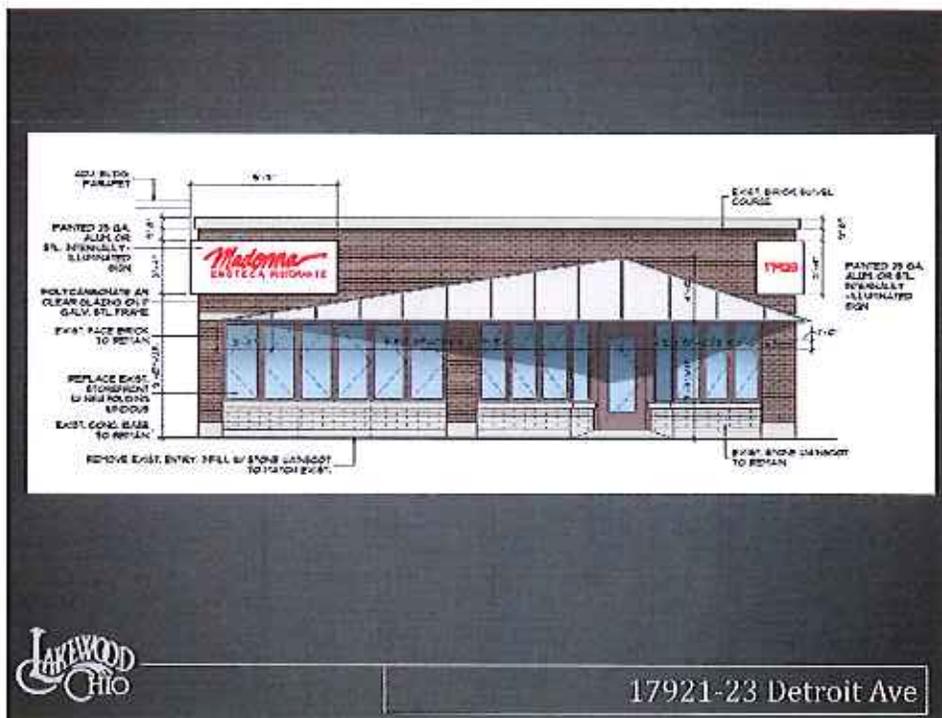
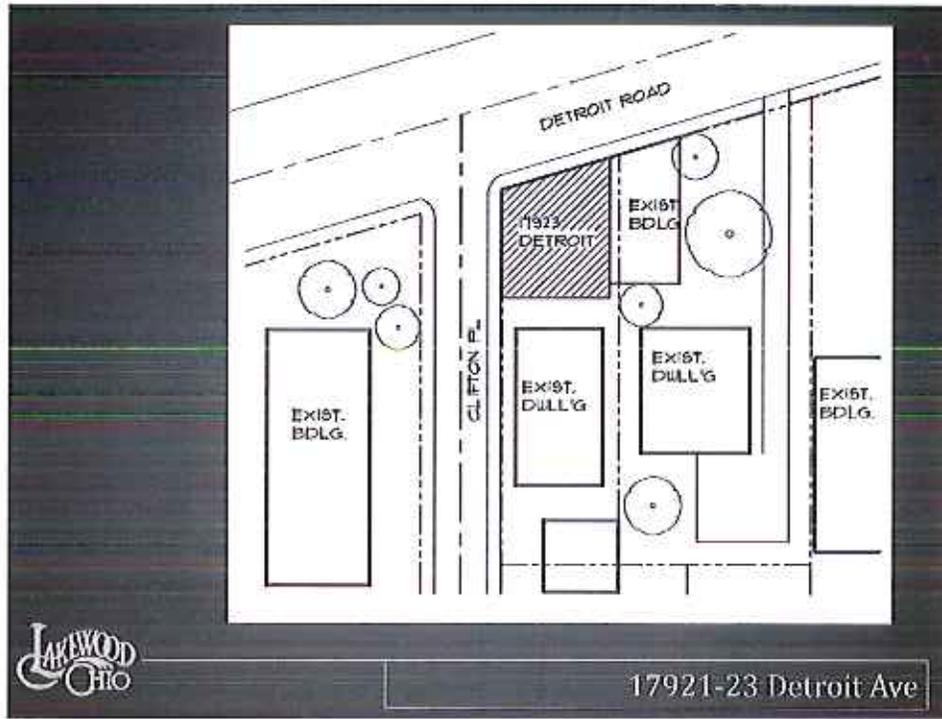




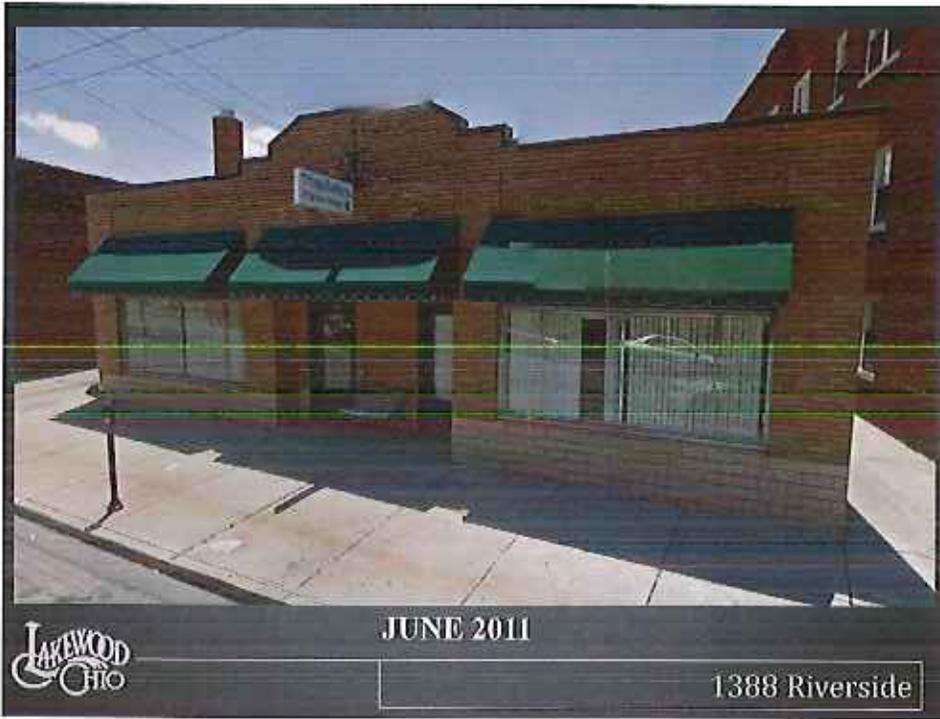


























LAKWOOD  
Ohio

14871 Detroit Ave



LAKWOOD  
Ohio

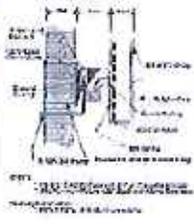
14871 Detroit Ave



20 ft. height  
30 sq. ft. aluminum



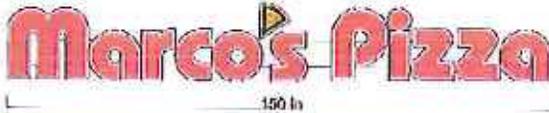
**Roofway Mounted Channel Letter**





38 in      7.9 sq ft

**30 Sq Ft. Total**



21.8 sq ft.  
150 in

Approved: *Kristin Cala*  
12/15/14

Order # MARCO'S PIZZA      Date Dec. 10, 2014      Phone 216-499-2500

Address Lakewood, Ohio      Installer Matt Huber



14871 Detroit Ave

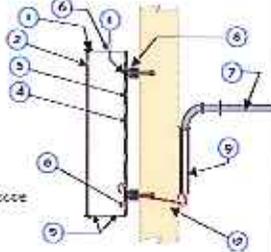


3'-4"      3'-0 1/4"      13'-6"      14'-3 1/2"

**PALM BEACH TAN**

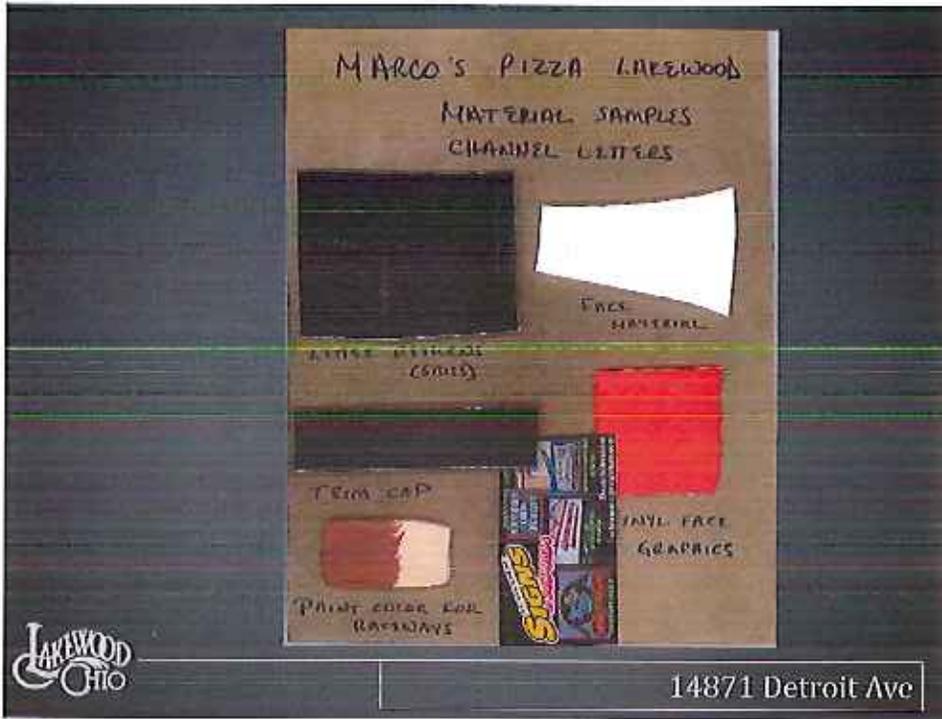
**LETTER DETAIL**

1. 1/8" CAP
2. 3/16" ACRYLIC FACES
3. ALUMINUM BACKGUSSET THICKNESS
4. LED LIGHT FIXTURES
5. DRAIN HOLES
6. ALUM. RETURN GIRD TO JOBS THICKNESS
7. POWER TO SIGN LOCATION BY OTHER
8. REQUIRED ANCHOR BOLTS
9. TRANSOM-INSTALL PER APPLIC. & NET CODE
10. EXTERIOR WALL
11. 2" X 2" ALUM. WINDOW





14879 Detroit

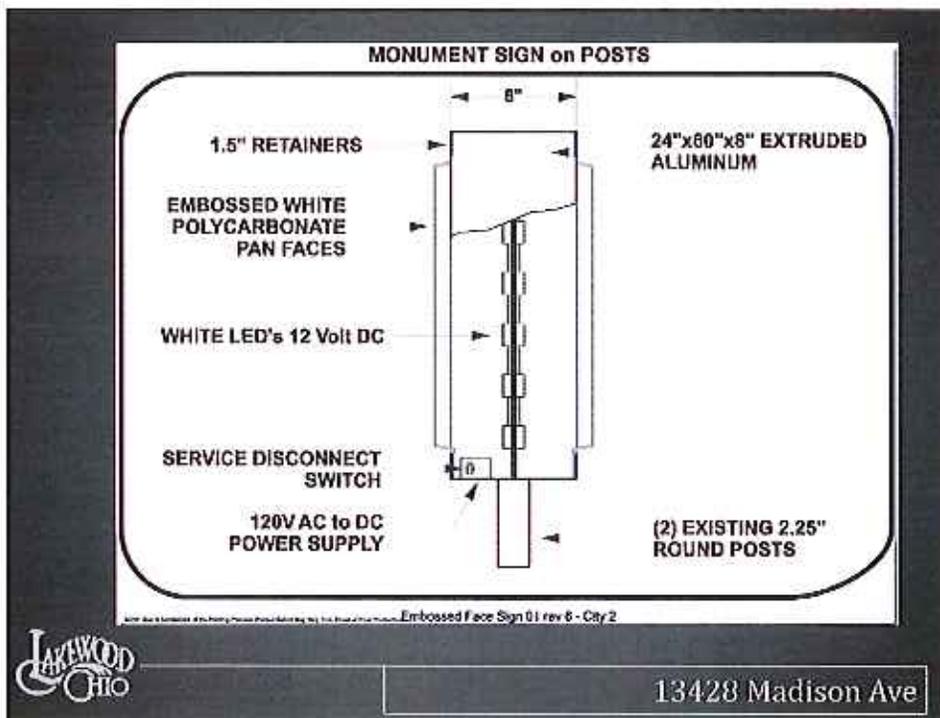




13428 Madison Ave



13428 Madison Ave



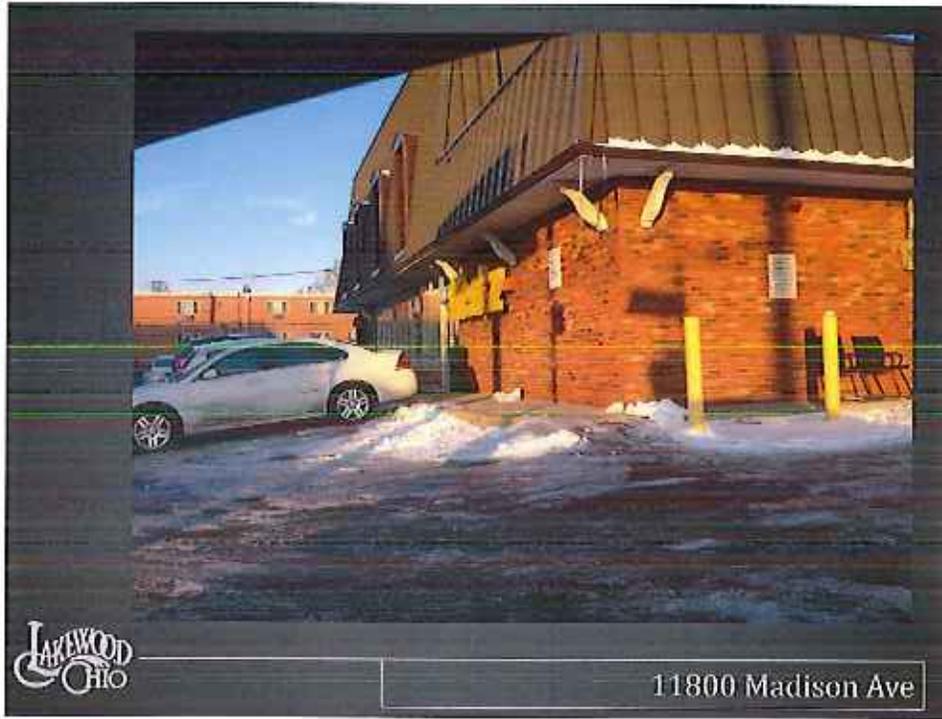


11800 Madison Ave



11800 Madison Ave





10'-2 1/2"

34 1/2"

20 1/2"

**Hertz**

GENESIS DESIGN  
SCALE: 1/2" = 1'-0"

SECTION  
R15

**LAKWOOD  
OHIO**

11800 Madison Ave

10'-8 1/2"

OUT SIDE - 115 7/8"

IN - 116 3/4"

4 1/2"

OUT SIDE - 115 7/8"

IN - 116 3/4"

**Hertz**

GENESIS DESIGN  
SCALE: 1/2" = 1'-0"

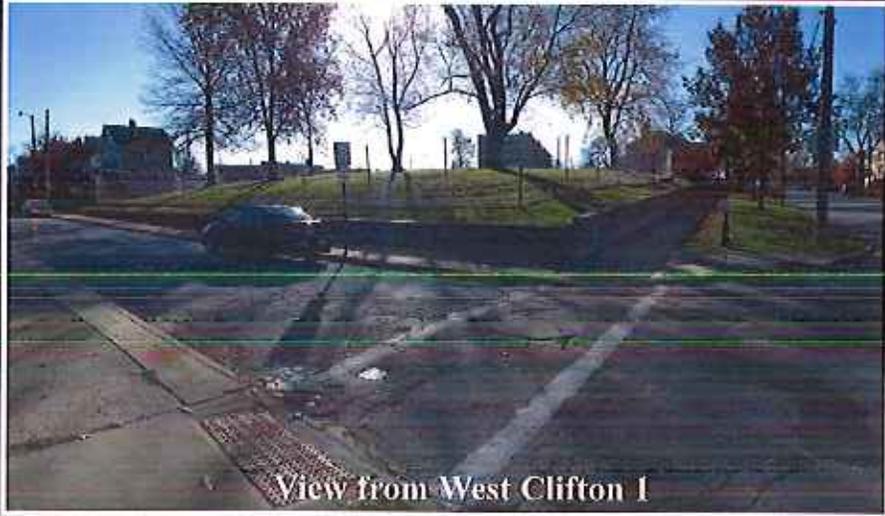
SECTION  
R15

**LAKWOOD  
OHIO**

11800 Madison Ave

**SCOPE:**  
 REPLACE EXISTING PYLON SIGN  
 PAN FOR NEW SIGN FACE  
 #2278 WHITE ACM/LC  
 1ST SURFACE APPLIED VINYL GRAPHIC  
 #4116 HERTZ YELLOW BACKGROUND  
 WITH BLACK COPY  
 RELAMP CABINET WITH  
 (4) 1100 COOL WHITE 4' 0"X LAMP  
 REPAINT COILING, REBARBERS, SUPPORT - WHITE





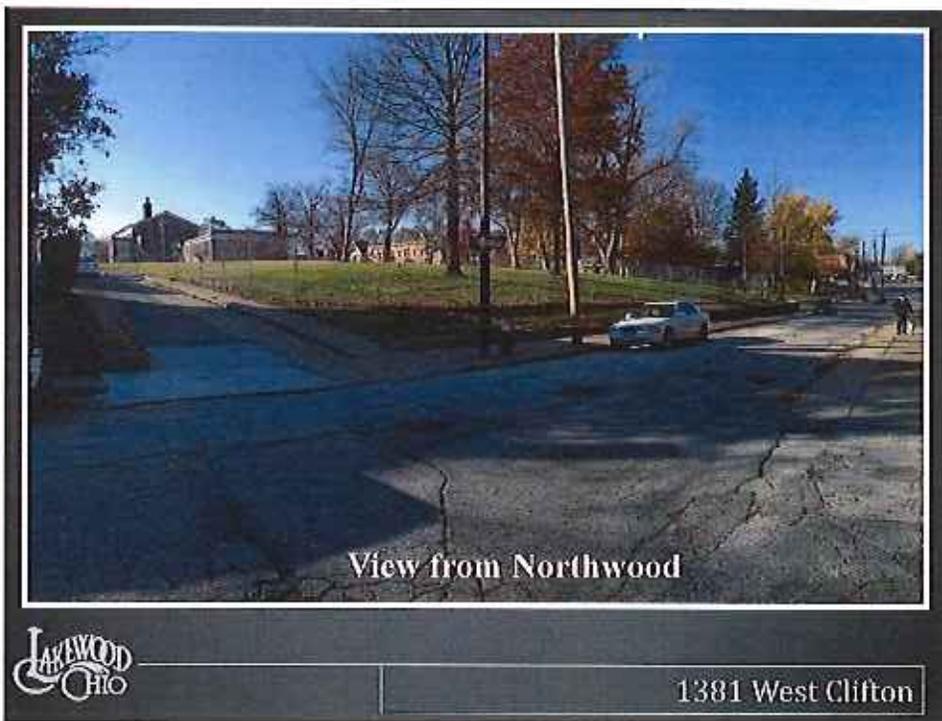
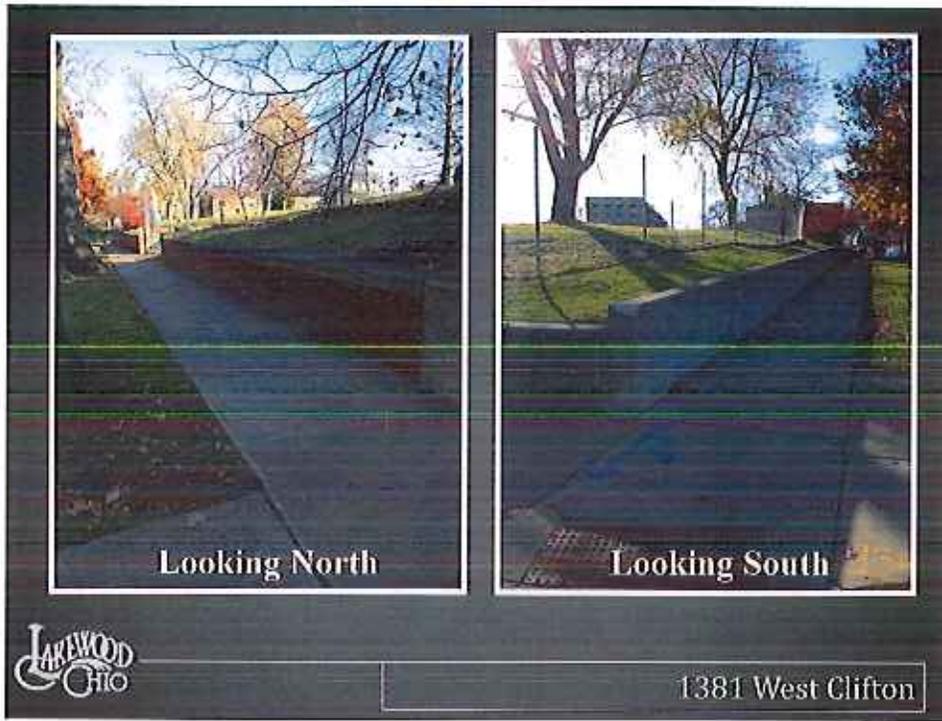
LAKWOOD  
OHIO

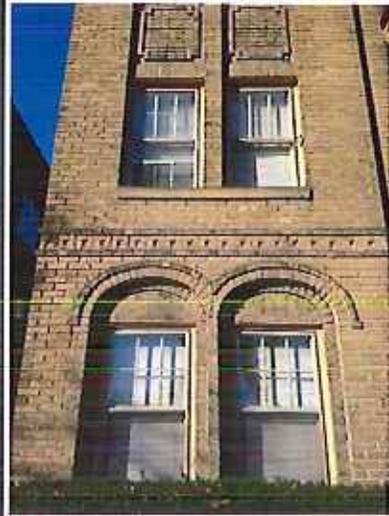
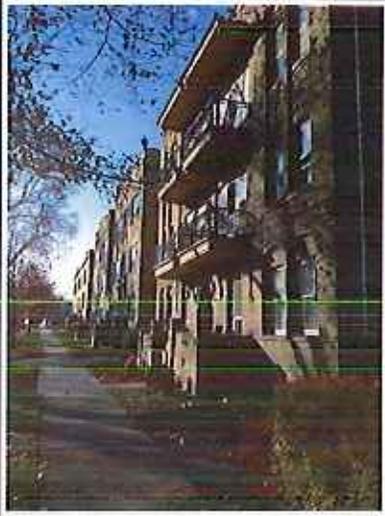
1381 West Clifton



LAKWOOD  
OHIO

1381 West Clifton

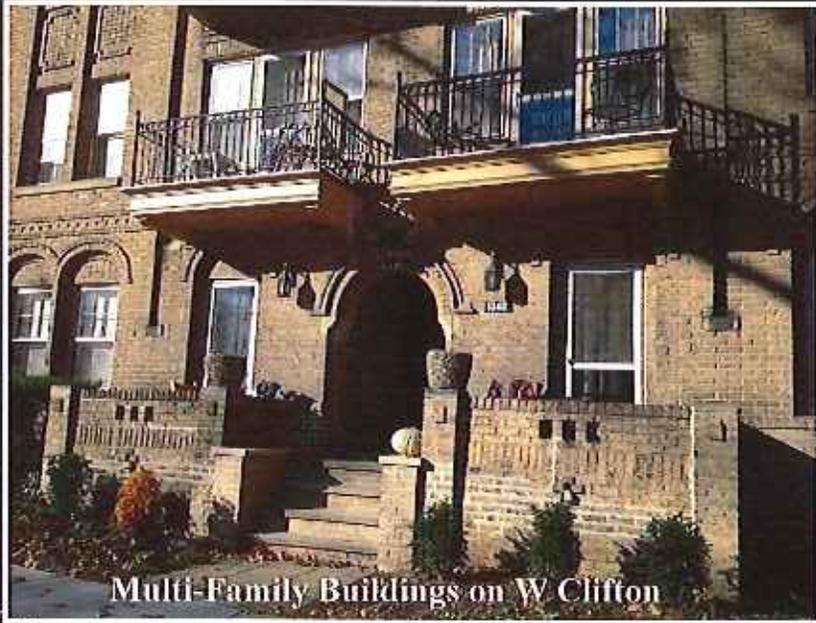




Multi-Family Buildings on W Clifton



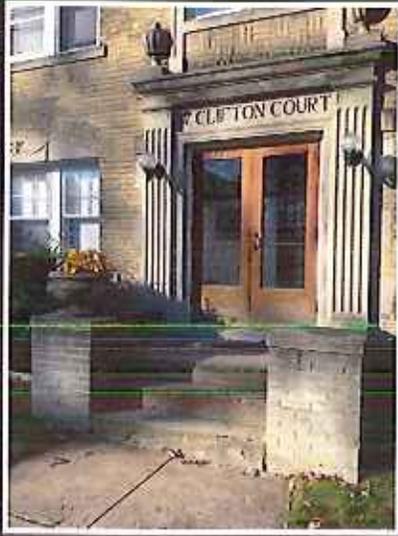
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Multi-Family Buildings on W Clifton



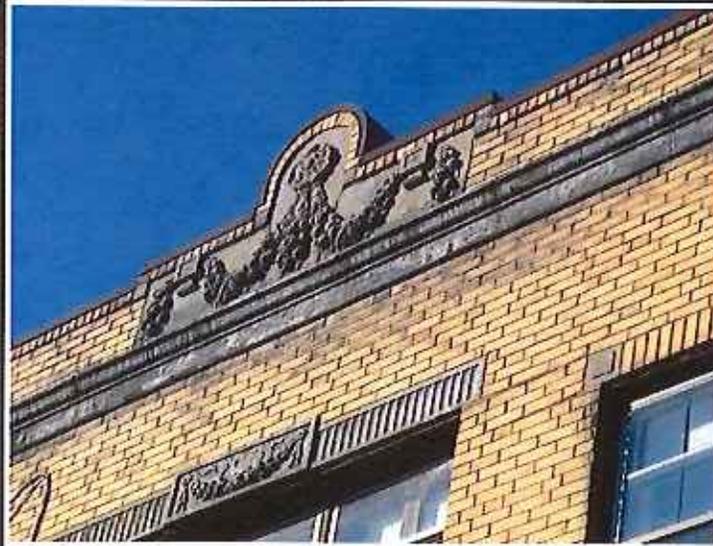
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Multi-Family Buildings on W Clifton



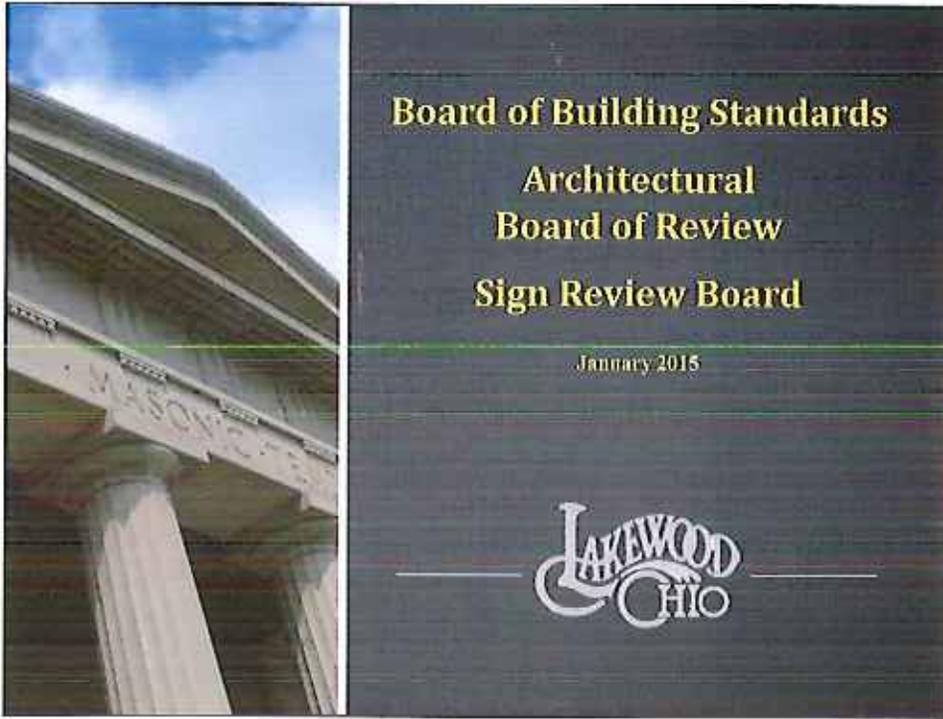
1381 West Clifton



Multi-Family Buildings on W Clifton



1381 West Clifton



**Board of Building Standards**  
**Architectural**  
**Board of Review**  
**Sign Review Board**

January 2015

LAKWOOD  
OHIO